

**AMENDED AGENDA
REGULAR MEETING
CITY COUNCIL, CITY OF ASHEBORO
THURSDAY, DECEMBER 5, 2013, 7:00 PM**

1. Call to order.
2. Moment of silent prayer and pledge of allegiance.
3. Presentation by W. Corey Basinger, Chairman, of the North Carolina Water Pollution Control System Operators Certification Commission, of the 2013 Wilbur E. Long, Jr. Operator of the Year Award to Chris Schadt, Wastewater Treatment Plant Lead Operator.
4. Report from Tammy O'Kelley, Director, Heart of North Carolina Visitors Bureau on Asheboro tourism activities for calendar year 2013.

OLD BUSINESS:

5. Consent agenda:
 - (a) Approval of the minutes of the city council's regular meeting that was held on November 7, 2013.
 - (b) Acknowledgement of the receipt of the minutes of the Asheboro ABC Board's meeting on October 7, 2013, including a copy of the required independent audit for the fiscal year ending June 30, 2013.
 - (c) Acknowledgement of the receipt of the Asheboro ABC Board's minutes of its meeting on November 4, 2013.
 - (d) Acknowledgement of the receipt from the Randolph County Board of Elections of a copy of the abstract of votes in the municipal general election held on November 5, 2013.

NEW BUSINESS:

6. Organizational meeting:
 - (a) Administration of the oaths of office for Mayor and Council Members.
 - (b) Election of Mayor Pro Tempore.
 - (c) Committee appointments:
 - (i) Appointments by Mayor
 - (ii) Council adoption of list of appointments

7. Consent agenda:

(a) Approval of an ordinance setting the Asheboro City Council's schedule for regular meetings during the 2014 calendar year.

(b) Approval of an ordinance establishing "No Parking Any Time" on the south side of Cooper Street from the intersection of Cooper Street and Lee Street to approximately 165 feet east of said intersection. (Requested by Orene M. Akers of 1003 Lee Street).

(c) Approval of a resolution authorizing the City Manager to enter into a Utility Agreement between the City of Asheboro and the NC Department of Transportation to replace water and sewer lines along Draper Street at the Penwood Branch crossing for the proposed NCDOT Bridge Replacement Project, wherein the estimated cost to the City is \$130,184.00, to be paid within sixty (60) days of receipt of invoice.

8. Community Development Division Items (Ms. Hemal Shah with NCDOT will present Item (a) and Mr. Nuttall will present Items (b), (c), and (d):

(a) Consideration of a request to adopt the Asheboro Comprehensive Transportation Plan prepared in conjunction with the North Carolina Department of Transportation:

- (i) Public hearing
- (ii) Action by the City Council on the request for adoption of the Asheboro Comprehensive Transportation Plan

(b) Zoning Case RZ-13-15: A legislative zoning hearing on the application filed by H.R. Gallimore (Agent for Nancy Jordan) to rezone property located east of Mountain Road, south of Springdale Lane, and west of Lincoln Avenue from CURA6 (Conditional Use High Density Residential) to RA6 (High Density Residential).

(c) Zoning Case RZ-13-16: A legislative hearing on the application filed by the City of Asheboro to amend Article 900 of the Asheboro Zoning Ordinance concerning the Board of Adjustment.

(d) Receipt of a report concerning the applications submitted for a vacant seat on the Asheboro Planning Board:

- (i) Review of Applications
- (ii) Authorization to forward submitted applications to the Randolph County Commissioners along with a recommendation for appointment to the Asheboro Planning Board as a representative from the city's extra territorial jurisdiction area

9. Public comment period.
10. Consideration of a resolution authorizing city officials to execute a memorandum of understanding between the city and GovDeals, Inc. that pertains to a proposed online auction process for surplus property. (The city attorney will present this item.)
11. Upcoming events:
 - Annual Christmas Parade begins at 7:00 PM on December 6, 2013.
 - "Christmas on Sunset" street festival begins at 6:00 PM on December 13, 2013.
 - Chamber of Commerce Christmas Party at the Exchange 5:30PM to 7:30PM on December 19, 2013.
 - Asheboro Fire Department Christmas Party at Station #1 from 6:00PM to 9:00PM on December 19, 2013.
 - Asheboro Police Department Christmas Party at PW Conference Room from 11:30 to 1:30PM on December 20, 2013.
12. Adjournment.

City of Asheboro
Water Resources
Division

Memo

To: John N. Ogburn, III
From: Michael D. Rhoney
CC:
Date: 12/2/2013
Re: December 5, 2013 Council Agenda Item

Chris Schadt, Wastewater Treatment Plant Lead Operator, has been awarded the 2013 Wilbur E. Long, Jr. Operator of the Year Award. The NC AWWA selects one municipal wastewater plant operator each year. W. Corey Basinger, Chairman, of the North Carolina Water Pollution Control System Operators Certification Commission will attend the December 5 council meeting to present the award.

The Wastewater Treatment Plant Operator of the Year Award was established in 1975 by the Wastewater Treatment Plant Operators Certification Commission. By Resolution, adopted by the Wastewater Treatment Plant Operators Certification Commission on January 12, 1977, the annual operator award was designated to be referred to henceforth as the Wilbur E. Long, Jr. Operator of the Year Award.

The purpose of the award is to recognize any properly certified Wastewater Treatment Plant Operator in North Carolina who has exhibited outstanding knowledge and innovation in the continuing operation of a wastewater treatment facility or who has contributed his time and efforts toward the training, education, and general upgrading of the Wastewater Treatment Plant Operator or who has devised, discovered or invented devices or techniques which enhance the science of plant operation.

City of Asheboro Council Meeting – Thursday, December 4, 2013

- **Update on 2013 American Legion Baseball Southeast Regional Tournament**

The American Legion Baseball Southeast Regional Tournament held at McCrary Park in August on August 7 – 12, 2013 was co-sponsored by the City of Asheboro & Heart of NC Visitors Bureau. The tournament included eight teams representing Kentucky, Virginia, North Carolina, South Carolina, Alabama, Georgia and Florida. The American Legion National Office projected an economic impact of the tournament to have generated \$750,000 and \$1,000,000 in visitor spending for the week.

2013 American Legion Baseball Southeast Regional Tournament

Occupancy (%)	AUG
	7th - 12th
This year	69.1
Last year	59.7
Percent Change	22.8
ADR	
This year	73.67
Last year	69.02
Percent Change	7.2
RevPAR	
This year	51.65
Last year	41.76
Percent Change	35.5
Demand	
This year	396
Last year	342
Percent Change	22.8
Revenue	
This year	177,572
Last year	143,555
Percent Change	35.5

- **Update on Cycle NC Event**

The 15th Annual "Mountains to Coast" Cycle NC ride was held Sept 28th - Oct 5th, 2013 and, again, co-sponsored by the City of Asheboro & Heart of NC Visitors Bureau. This year's route started in Spruce Pine and ended in Atlantic Beach, with an overnight stop in Asheboro on October 1st. There were 1,055 cyclists from 35 states, the District of Columbia, the Virgin Islands and Canada who participated, and an additional approximate 200 support staff. Hosted by North Carolina Amateur Sports (NCAS), the economic impact of spending one night in Asheboro is projected to have generated \$100,000 in visitor spending.

Cycle NC	
Occupancy (%)	Sep 30th & Oct 1st
This year	66.0
Last year	42.7
Percent Change	53.8
ADR	
This year	70.46
Last year	65.95
Percent Change	6.8
RevPAR	
This year	46.71
Last year	28.18
Percent Change	64.5
Demand	
This year	378
Last year	245
Percent Change	53.8
Revenue	
This year	53,535
Last year	32,289
Percent Change	64.5

**REGULAR MEETING
ASHEBORO CITY COUNCIL
CITY COUNCIL CHAMBER, MUNICIPAL BUILDING
THURSDAY, NOVEMBER 7, 2013
7:00 p.m.**

This being the time and place for a regular meeting of the Asheboro City Council, a meeting was held with the following officials and staff members present:

- David H. Smith) – Mayor Presiding
- Talmadge S. Baker)
- Clark R. Bell)
- Edward J. Burks)
- Linda H. Carter) – Council Members Present
- Michael W. Hunter)
- Walker B. Moffitt)
- Charles A. Swiers)

John N. Ogburn, III, City Manager
 Holly H. Doerr, CMC, NCCMC, City Clerk/Paralegal
 John L. Evans, Assistant Community Development Director
 Michael L. Leonard, P.E., City Engineer
 Ralph W. Norton, Chief of Police
 Trevor L. Nuttall, Community Development Director
 Deborah P. Reaves, Finance Director
 Michael D. Rhoney, Water Resources Director
 Charles E.L. Spencer, Police Sergeant
 Jeffrey C. Sugg, City Attorney
 E. Todd Swaney, Police Captain

1. Call to order.

A quorum thus being present, Mayor Smith called the meeting to order for the transaction of business, and business was transacted as follows.

2. Silent prayer and pledge of allegiance.

After a moment of silence was observed in order to allow for prayer or meditation, Mayor Smith asked everyone to stand and repeat the pledge of allegiance.

3. Approval of a resolution recognizing the 30th Anniversary of the founding of Hospice of Randolph County.

Upon motion by Ms. Carter and seconded by Mr. Baker, Council voted unanimously to adopt the following resolution by reference.

48 RES 11-13

Resolution

National Hospice Palliative Care Month – November 2013

Whereas, hospice and palliative care empower people to live as fully as possible, surrounded and supported by family and loved ones, despite serious and life-limiting illness; and

Whereas, hospice and palliative care bring patients and family caregivers the highest quality care delivered by an interdisciplinary team of skilled professionals that includes physicians, nurses, social workers, therapists, counselors, health aides, spiritual care providers and others who make the wishes of each patient and family a priority; and

Whereas through pain management and symptom control, caregiver training and assistance, and emotional and spiritual support, allowing patients to live fully up until the final moments, surrounded and supported by the faces of loved ones, friends, and committed caregivers; and

Whereas, 2013 is the 30th anniversary of the founding of Hospice of Randolph County; and

Whereas hospice and palliative care providers encourage all people to learn more about options of care and to share their wishes with family, loved ones, and their healthcare professionals;

12, 2013, and on October 10, 2013. After receiving sworn testimony and considering all of the evidence, including a proposed site plan, presented during the quasi-judicial hearing, the Council, on the basis of competent, material, and substantial evidence, hereby resolves the question of the issuance of the requested Special Use Permit by entering the following:

FINDINGS OF FACT

1. PF-Asheboro Too, LLC (hereinafter referred to as the "Applicant") has submitted an application for a Special Use Permit that would authorize the development of a commercial planned unit development on approximately 78,515 square feet (1.8 acres) of land currently owned by the Applicant.

2. The above-referenced land is located at 1280 East Dixie Drive and consists of a single parcel of land that is more specifically identified by Randolph County Parcel Identification Number 7760596184. This parcel of land will be hereinafter referred to as the "Zoning Lot."

3. The Zoning Lot is an undeveloped commercial parcel of land surrounded by commercial land uses to the north, south, east and west.

4. The Zoning Lot is located within a B2 (General Commercial) zoning district.

5. Under the Asheboro Zoning Ordinance, a commercial planned unit development can be developed in a B2 zoning district so long as such development is conducted in accordance with an approved Special Use Permit (hereinafter referred to as the "SUP").

6. The Growth Strategy Map identifies the area in which the Zoning Lot is located as a "Primary Growth" area, and the Proposed Land Development Plan Map designates this location as a "Commercial" area.

7. The Zoning Lot is located within the corporate limits of the City of Asheboro.

8. The Applicant is seeking approval for a commercial planned unit development (hereinafter referred to as a "PUD") with two lots because, as an approved use, the PUD concept would allow for the subdivision of the Zoning Lot into two lots that are each subject to conveyance as lots within the approved PUD.

9. Due to the significant compliance issues associated with subdividing the Zoning Lot independent of an approved PUD in a situation such as this one where one of the subdivided lots within the PUD will not be contiguous with a public street, the Council has examined whether the proposed use is viable as a functioning PUD. A private drive, which is to be maintained by a homeowners' association, will provide cross-access between the lots within the requested PUD. Both of the lots within the PUD will have access via privately maintained streets/driveways that connect, with the legal protection of easements, to a public right-of-way (East Dixie Drive).

10. The proposed subdivision of the Zoning Lot, as part of the requested PUD, can be accomplished through the process known under the city's land development regulations as a minor subdivision. The proposed subdivision is deemed to be minor as opposed to major because the proposal does not implicate any new dedication of public right-of-way or the construction of water/sewer lines to be maintained by the City of Asheboro. Minor subdivisions are subject to administrative review by city staff.

11. While the Zoning Lot is currently undeveloped, a car dealership was formerly located in this area. On the basis of the evidence presented, no elevated risk of any type of harm can be discerned from creating a 2-lot commercial PUD on this land that is currently surrounded by commercial uses and that has been the site of an active automobile dealership.

12. With the conditions suggested by the city's community development division staff, the site plan submitted by the Applicant is in compliance with the prescribed regulations and specifications of the Asheboro Zoning Ordinance. The conditions offered for attachment to a potential SUP are as follows:

(A) The label on the site plan denoting the total common area shall be amended to read: "Private Drive within common area: totals 3,300 square feet (0.0758 AC)."

(B) A property owners' association shall be established to ensure, at a minimum, maintenance of the private drive and common area referenced above in Condition (A).

(C) Maintenance of the sidewalk within the common area and on privately owned property shall be the responsibility of the property owners' association and individual property owner(s), respectively.

(D) The site plan depicts landscaping that is required within the common area, and landscaping within the front yard of Lot 2(A) adjacent to East Dixie Drive and the drive labeled "access drive." Landscaping adjacent to the privately maintained access drive (labeled "access drive" on the approved site plan) shall include, at a minimum, 8 canopy trees consistent with the landscaping that is shown on the approved site plan. Other required landscaping shall meet the requirements of the Asheboro Zoning Ordinance based on the landscaping requirements for each specific use as each lot develops.

(E) Prior to the approval of a subdivision plat for the proposed lots, the owner(s) of the Zoning Lot shall properly execute for recordation in the office of the Randolph County Register of Deeds a Memorandum of Land Use Restrictions prepared by the City Attorney for the purpose of placing notice of the conditions attached to this Special Use Permit in the chain of title for the Zoning Lot.

13. Mr. Ben Morgan, Esq. represented the Applicant during the hearing of this matter, and he informed the Council of his client's decision to accept and comply with the conditions recommended by the community development division staff.

14. No testimony was offered in opposition to the Applicant's request.

15. Pursuant to Section 602.1 of the Asheboro Zoning Ordinance, the Council must find that the Applicant has met four (4) general standards before an application for a SUP may be approved. The four standards are as follows:

(A) That the use will not materially endanger the public health or safety if located where proposed and developed according to the plan as submitted and approved.

(B) That the use meets all required conditions and specifications.

(C) That the use will not substantially injure the value of adjoining or abutting property, or that the use is a public necessity.

(D) That the location and character of the use if developed according to the plan as submitted and approved will be in harmony with the area in which it is to be located and in general conformity with the plan of development of Asheboro and its environs.

Based on the foregoing findings of fact, the Council hereby enters the following:

CONCLUSIONS OF LAW

1. When an applicant has produced competent, material, and substantial evidence tending to establish the existence of the facts and conditions that the Ordinance requires for the issuance of a SUP, prima facie the applicant is entitled to the permit. A denial of the permit has to be based upon findings contra that are supported by competent, substantial, and material evidence appearing in the record.

2. In this case, the Applicant has properly submitted an application for a SUP authorizing the development of a commercial PUD on the Zoning Lot that is located in a B2 zoning district.

3. In light of the available evidence and the express agreement of the Applicant to accept and comply with the conditions recommended by the Community Development Division staff, the Applicant's site plan for the proposed land use is compliant with the applicable requirements of the Asheboro Zoning Ordinance.

4. On the basis of the evidence presented during the hearing of this matter, the Council has concluded that the proposed use meets the four (4) general standards for the granting of a SUP. More specifically, the proposed use will not materially endanger the public health or safety, meets all required conditions and specifications of the zoning ordinance, will not substantially injure the value of adjoining or abutting property, and will be in harmony with the area in which it is to be located and is in general conformity with Asheboro's plan of development.

Based on the above-recited findings of fact and conclusions of law, the Council hereby enters the following:

ORDER

The Applicant, PF-Asheboro Too, LLC, is hereby issued a Special Use Permit authorizing the development of a commercial planned unit development, as defined by the Asheboro Zoning Ordinance, on the Zoning Lot. This Special Use Permit shall be valid so long as, and only so long as, PF-Asheboro

30 ORD 11-13

AN ORDINANCE CLARIFYING THE ONE-WAY STREET REGULATIONS APPLICABLE TO THE ENTIRETY OF SCARBORO STREET AND A SECTION OF HOLLY STREET

WHEREAS, Section 160A-296 of the North Carolina General Statutes provides that a city shall have general authority and control over all public streets, sidewalks, alleys, bridges, and other ways of public passage within its corporate limits; and

WHEREAS, the Code of Asheboro, in Section 70.43 (titled "Operation on One-Way Streets"), provides as follows:

Upon those streets and parts of streets described in schedule 8 of § 72.02, vehicular traffic shall move only in the indicated direction when authorized signs indicating the direction of traffic are erected and maintained at every intersection where movement in the opposite direction is prohibited; and

WHEREAS, the Code of Asheboro, in Section 70.21 (titled "One-Way Street Signs"), provides as follows:

Whenever authorized signs are placed, erected or installed indicating that traffic shall proceed only in a certain direction, no driver of a vehicle shall disobey the directions contained in or given by such signs; and

WHEREAS, the Code of Asheboro, in Subsection (C) of Section 70.29 (titled "Designation and Posting Signs or Marking Streets"), provides as follows:

Pursuant to instructions given to him by the City Council from time to time and entered in the council minute book, the City Manager shall:

(C) Cause all streets and parts of streets which are designated as one-way streets to be posted accordingly and so as to show the direction in which traffic shall proceed. He shall then notify the City Clerk, who shall enter the description of such streets and parts of streets, and the direction of traffic thereon, in schedule 8 of § 72.02; and

WHEREAS, prior to the adoption of this ordinance, Scarboro Street has been recognized and posted for an extended period of time as a one-way street with traffic entering from South Fayetteville Street and proceeding eastward; and

WHEREAS, a recent search, which was initiated by the City Clerk's office as a matter of records management, revealed that the above-referenced schedule 8 does not reflect the long-standing designation of Scarboro Street as a one-way street; and

WHEREAS, contemporaneous with the records research pertaining to Scarboro Street, the discovery was made that, when the section of Holly Street on the south side of Loflin Elementary School was designated in 1959 as one-way during school hours, the description entered into the schedule of one-way streets listed Hammer Avenue as the point of entry for the one-way traffic on Holly Street; and

WHEREAS, in reliance upon the recommendation of city staff, the Asheboro City Council has concluded that the city's records should be updated to clearly and accurately reflect the one-way traffic patterns that have been established and are deemed to be appropriate for Scarboro Street and a section of Holly Street;

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Asheboro as follows:

Section 1. Scarboro Street is hereby designated as one-way for vehicular traffic during all times of the day with traffic entering Scarboro Street from South Fayetteville Street (United States Highway 220 Business), and, while traveling on Scarboro Street, traffic is prohibited from proceeding in any direction on the street other than eastward toward the intersection with South Cox Street (North Carolina Secondary Road 2327).

Section 2. From 7:15 a.m. to 8:15 a.m. and from 2:00 p.m. to 3:00 p.m. on school days, the section of Holly Street between South Church Street (North Carolina Secondary Road 1107) and South Park Street (North Carolina Secondary Road 1451) is hereby designated as one-way for vehicular traffic with entry onto this section of the street from South Church Street, and, while traveling on this section of Holly Street during the designated times on school days, traffic is prohibited from proceeding in any direction on the street other than westward toward the intersection with South Park Street.

Section 3. The City Manager is hereby instructed to cause the proper installation of signage, as well as the placement of any other form or type of traffic control markings and/or device(s) deemed appropriate by the City Manager, on the streets and at the intersections referenced in Sections 1 and 2 of

this Ordinance in order to indicate the direction of travel for vehicular traffic that is prohibited as a consequence of the adoption of this Ordinance.

Section 4. The City Clerk shall properly record the one-way street designations made by this Ordinance in schedule 8 of Section 72.02 of the Code of Asheboro.

Section 5. All ordinances and clauses of ordinances in conflict with this ordinance are hereby repealed.

Section 6. This ordinance shall take effect and be in force from and after the date of its adoption.

This ordinance was adopted in open session during a regular meeting of the Asheboro City Council that was held on November 7, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

- (g) **A resolution authorizing the transfer of a lease agreement pertaining to Hangar I at the Asheboro Regional Airport from Matlab, Inc. to Polyspray Corporation.**

49 RES 11-13

RESOLUTION AUTHORIZING THE TRANSFER OF AN AIRPORT HANGAR LEASE FROM MATLAB, INC. TO POLYSPRAY CORPORATION

WHEREAS, Section 160A-272 of the North Carolina General Statutes, as modified by the local modification found in Chapter 867 of the 1989 (Reg. Sess., 1990) Session Laws of North Carolina, authorizes the City Council of the City of Asheboro to lease, in relation to the operation of the Asheboro Regional Airport, city-owned property located at said airport for a maximum lease term of thirty (30) years so long as the property will not be needed by the municipality during the term of the lease; and

WHEREAS, in response to a request received from the vice president and secretary for Matlab, Inc., the Asheboro Airport Authority has recommended approval of the request by Matlab, Inc. to transfer the existing lease agreement for Hangar I at the Asheboro Regional Airport from Matlab, Inc. to Polyspray Corporation; and

WHEREAS, the city attorney has prepared two (2) instruments that, if approved, would implement the leasing arrangements recommended by the Asheboro Airport Authority for Hangar I; and

WHEREAS, the instruments referenced in the immediately preceding paragraph are a TERMINATION OF LEASE AGREEMENT, which is attached to this resolution as EXHIBIT 1, and a LEASE OF HANGAR AT ASHEBORO REGIONAL AIRPORT agreement, which is attached to this resolution as EXHIBIT 2; and

WHEREAS, Hangar I will not be needed by the city during the term of the proposed lease with Polyspray Corporation; and

WHEREAS, public notice of the intent to approve the transfer of the lease agreement to Polyspray Corporation was given in *The Courier-Tribune* on October 24, 2013;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Asheboro that the transfer of the lease agreement pertaining to Hangar I at the Asheboro Regional Airport from Matlab, Inc. to Polyspray Corporation in accordance with the terms and conditions specified in EXHIBIT 1 and EXHIBIT 2, which are hereby incorporated into this Resolution by reference as if copied fully herein, is hereby authorized; and

BE IT FURTHER RESOLVED that, in order to implement the approved lease transfer, the Mayor and City Clerk are hereby authorized and directed to execute any and all instruments necessary to consummate the transaction reflected in EXHIBIT 1 and EXHIBIT 2.

This resolution was adopted by the Asheboro City Council in open session during a regular meeting held on the 7th day of November, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

[The exhibits referenced in the adopted Resolution are on file in the City Clerk's office.]

- (h) **A resolution approving proposed amendments of the Municipal Records Retention Disposition Schedule that was initially published on September 10, 2012.**

50 RES 11-13

A RESOLUTION APPROVING AMENDMENTS OF THE MUNICIPAL RECORDS RETENTION AND DISPOSITION SCHEDULE FOR THE CITY OF ASHEBORO

WHEREAS, in accordance with Section 121-5 of the North Carolina General Statutes and Section 132-3 of the North Carolina General Statutes, public records maintained by the City of Asheboro may only be destroyed with the consent of the North Carolina Department of Cultural Resources; and

WHEREAS, the Municipal Records Retention and Disposition Schedule (the "Retention Schedule") issued by the Government Records Branch of the State Archives of North Carolina, which is part of the Division of Archives and Records within the North Carolina Department of Cultural Resources, is the primary means by which the Department of Cultural Resources gives its consent for the destruction of records that do not and will not have further use or value for official business, research, or reference purposes after the respective retention periods specified within the schedule; and

WHEREAS, in the absence of approving the Retention Schedule, the city is obligated to obtain permission from the Department of Cultural Resources to destroy any record, regardless of how insignificant a particular record might be; and

WHEREAS, prior to the City Council's consideration of this Resolution, the most recent edition of the Retention Schedule, which was approved by the City Council in 2012, was published on September 10, 2012; and

WHEREAS, in August 2013, the Department of Cultural Resources published amendments to Standard 4 (Budget, Fiscal, and Payroll Records) and Standard 12 (Personnel Records) of the Retention Schedule; and

WHEREAS, subsequent to the publication of the Retention Schedule in September 2012, city staff completed the preparation of policies specific to the City of Asheboro for the purpose of defining the minimum retention period for records that the Department of Cultural Resources scheduled with the instruction "destroy when administrative value ends;" and

WHEREAS, a revised Retention Schedule (the "Amended Retention Schedule") containing the above-referenced amendments promulgated by the North Carolina Department of Cultural Resources along with the policies generated by city staff to set minimum retention periods for records subject to the instruction to "destroy when administrative value ends" has been attached to this instrument as EXHIBIT 1, and this exhibit is hereby incorporated into this Resolution by reference as if copied fully herein; and

WHEREAS, the City Clerk and City Manager have recommended approval of the Amended Retention Schedule;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Asheboro that the Amended Retention Schedule attached hereto as EXHIBIT 1 is hereby approved; and

BE IT FURTHER RESOLVED that the Mayor is hereby authorized to execute the signature page for the approved Amended Retention Schedule that is attached hereto as EXHIBIT 2, and this exhibit is hereby incorporated into this Resolution by reference as if copied fully herein.

This Resolution was adopted by the Asheboro City Council in open session during a regular meeting held on the 7th day of November, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

[The exhibits referenced in the adopted Resolution are on file in the City Clerk's office.]

(i) Acknowledgement of the receipt of the Asheboro ABC Board's minutes of its meeting September 3, 2013.

Copies of the minutes of the Asheboro ABC Board's meeting on September 3, 2013 were received by the Mayor and Council Members, and copies of these minutes are on file in the City Clerk's office.

(j) A resolution authorizing a copier rental agreement.

51 RES 11-13

RESOLUTION APPROVING AN EQUIPMENT RENTAL AGREEMENT

WHEREAS, the City of Asheboro has a total of 4 copiers that need to be replaced in order to meet the needs of certain departments, specifically water billing, engineering, fleet maintenance, and the water treatment plant; and

WHEREAS, the offices served by these copiers do not generate a sufficient volume of copies to obtain new copiers on the basis of a per copy fee arrangement that is cost effective; and

WHEREAS, the current supplier, Applied Copier Concepts, has offered to supply, in conjunction with EverBank Commercial Finance, Inc., 4 used copiers that will meet the city's requirements, including added networking capability and a service agreement, at a monthly rental rate for all 4 machines of \$160.00 plus a per copy charge of \$0.013 for a term of 48 months; and

WHEREAS, due to the combination of Applied Copier Concept's willingness to absorb the shipping cost associated with removing their current copiers from city facilities and the fact that the above-described charges cover supplies, other than paper, and repair work during the 48-month term of the proposed rental agreement, city staff members have determined that the proposed arrangement compares favorably with any other option that might be available to the city; and

WHEREAS, EverBank Commercial Finance, Inc. has agreed to modify its standard rental agreement to conform with the city's procurement and contracting practices; and

WHEREAS, in order to move forward with this proposal, the proposed rental agreement, as amended to conform with the city's contracting requirements, must be approved by the City Council; and

WHEREAS, the said amended rental agreement has been attached to this Resolution as EXHIBIT 1 and is hereby incorporated into this Resolution by reference as if copied fully herein;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Asheboro that the proposed equipment rental agreement attached to this Resolution as EXHIBIT 1 is hereby approved; and

BE IT FURTHER RESOLVED by the City Council of the City of Asheboro that the City Manager and his designees are hereby authorized to execute on behalf of the City all documents and instruments necessary to carry into full force and effect the said equipment rental agreement.

This resolution was adopted by the Asheboro City Council in open session during a regular meeting held on the 7th day of November, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

[The exhibits referenced in the approved Resolution is on file in the City Clerk's office.]

6. Community Development Division items:

- (a) Zoning Case RZ-13-13: A public hearing on the application filed by Patsy M. Hinkle to rezone the property located at 163 East Academy Street from OA6 (Office-Apartment) to B3 (Central Commercial).**

Mayor Smith opened the public hearing on the following request.

The requested rezoning pertains to approximately 0.27 of an acre of land owned by Patsy M. Hinkle and located at 163 East Academy Street. This property is more specifically identified by Randolph County Parcel Identification Number 7751816945.

Mr. Nuttall presented the Community Development Division staff's analysis of the request submitted by Mr. H.R. Gallimore, who is acting as the agent for Ms. Patsy M. Hinkle, to rezone the above-described property from OA6 (Office-Apartment) to B3 (Central Commercial)

The staff report noted that the property is located within Tier 2 of the Center City Planning Area, and the area is surrounded by a mix of office/service and multi-family residential uses along East Academy Street. More intense commercial uses are further west of the subject property along South Fayetteville Street. Placing the property in the B3 zoning district would allow all uses permitted by right in the district. Two key differences between the B3 district and the OA6 district are (1) a B3 district does not require off-street parking and (2) commercial uses such as restaurants and retail are permitted in the B3 district.

The Planning Board concurred with the following Community Development Division staff's analysis and recommended approval of the requested rezoning:

"The LDP proposed land use map designates the property as part of the Center City Planning Area (CCPA), which envisions a larger-scale mixed-use activity center serving the entire community. The Center City Planning Area map more precisely designates this property within Tier 2 of the CCPA. Tier 2 is distinguished by less availability of public parking than Tier 1, which raises a concern about the request since the B3 district requires no off-street parking.

There are, however, a number of factors that support the request. Although the property is located in Tier 2, it is contiguous to Tier 1 (to the north). On-street parking is available immediately in front of the property and in most of the surrounding area. There is B3 zoning approximately sixty-five feet west of the subject property and CUB3 zoning southeast of the subject property. Additionally, references throughout the LDP, including Design Principal #3 encourage a mix of uses to allow residents, especially the very old and very young, to be able to walk or bike to the store or work." Designating property B3 contiguous to the existing CBD and in such close proximity to high-density residential uses reduces the need for off-street parking and encourages modes of alternate transportation, such as walking and bicycling. Additionally, development requirements within Tier 2 and of the CCPA, such as front yard setback averaging and requiring that off-street parking (when provided) is located to the side and rear of buildings, will ensure that the property develops consistently with development patterns downtown."

Mr. H.R. Gallimore was available to answer questions.

There being no further comments and no opposition from the public, Mayor Smith transitioned to the deliberative phase of the public hearing.

Upon motion by Bell and seconded by Ms. Carter, Council voted unanimously to adopt the recommendation/analysis of the Community Development Division staff and the Planning Board and approved the requested rezoning as well as adopting the following consistency statement:

In consideration of the above factors, the zoning map amendment will allow a reasonable use of the property and ensure consistency with the Land Development Plan.

(b) Zoning Case RZ-13-14: A public hearing on the application by Garrett Smith to rezone the property located at 2128 and 2148 U.S. Highway 64 East from CU-B2 (Conditional Use General Commercial) to B2 (General Commercial).

Mayor Smith opened the public hearing on the following request.

The requested rezoning pertains to approximately 3.98 acres of land owned by McKenzie Properties Investment, LLC and located at 2128 and 2148 U.S. Highway 64 East. This property is more specifically identified by Randolph County Parcel Identification Number 7771046351.

Mr. Nuttall presented the Community Development Division staff's analysis of the request by Triad Corrugated Metal, by and through Mr. Garrett Smith, to rezone the above-described property from CUB2 (Conditional Use General Commercial) to B2 (General Commercial).

The staff report noted the following:

1. The property is located outside of the city limits. City water is available to the property, but city sewer is not currently available to the property.
2. U.S. Highway 64 East is a state-maintained major thoroughfare, and Luck Road is a state-maintained minor thoroughfare.
3. The U.S. Highway 64 East corridor has seen a transition from residential uses to commercial uses in recent years. It is likely that trend will continue.
4. The current zoning (CUB2) and conditional use permit only allow for sales of manufactured homes and related activities. This use has been discontinued.
5. The Zoning Ordinance Statement of Intent (Section 210) states that the B2 district "is intended to serve the convenience goods, shoppers goods retail and service needs of the motoring public, both local and transient. This district should always be located with access directly to major or minor thoroughfares, never local streets."
6. Zoning the property B2 would permit all uses allowed by right in the district.

The Planning Board concurred with the following Community Development Division staff's analysis and recommended approval of the requested rezoning

"This property is designated as commercial by the Land Development plan map. Since the property was originally zoned CUB2 (Conditional Use General Commercial), the U.S. Highway 64 corridor has become more commercial in nature and the adjacent property to the south (currently in Randolph County's zoning jurisdiction) has been rezoned for industrial use, creating less need for a conditional use commercial district to ensure compatibility with the surrounding area.

Additionally, the LDP East Small area plan recognizes the increasing commercial significance of the U.S. Highway 64 East corridor."

Mr. Garrett Smith, a representative of Triad Corrugated Metal, presented comments in support of the rezoning.

There being no further comments and no opposition from the public, Mayor Smith transitioned to the deliberative phase of the public hearing.

Upon motion by Mr. Moffitt and seconded by Mr. Burks, Council voted unanimously to adopt the recommendation/analysis of the Community Development Division staff and the Planning Board and approved the requested zoning as well as adopting the following consistency statement:

In consideration of the above factors, the zoning map amendment will allow a reasonable use of the property and ensure consistency with the Land Development Plan.

(c) A public hearing on a proposal to modify the size and composition of the Asheboro Planning Board by adopting an ordinance amending Chapter 34 (Departments, Boards, and Commissions) of the Code of Asheboro.

Mayor Smith opened the public hearing on the proposal to modify the size and composition of the Asheboro Planning Board.

Mr. Nuttall presented and recommended adoption by reference of an ordinance amending Chapter 34 (Departments, Boards, and Commissions) of the Code of Asheboro. There being no comments and no opposition from the public, Mayor Smith transitioned to the deliberative phase of the hearing.

Upon motion by Mr. Bell and seconded by Mr. Hunter, Council voted unanimously to adopt the following ordinance by reference.

31 ORD 11-13

**AN ORDINANCE AMENDING CHAPTER 34 (DEPARTMENTS, BOARDS,
AND COMMISSIONS) OF THE CODE OF ASHEBORO**

WHEREAS, consistent with the authority granted to the city by virtue of Chapter 160A (Cities and Towns), Article 19 (Planning and Regulation of Development) of the North Carolina General Statutes, the Asheboro City Council, after giving proper notice and conducting a public hearing over the course of two (2) regular meetings, redrew the extraterritorial planning jurisdiction boundary line for the City of Asheboro by adopting Ordinance No. 36 ORD 12-12 on December 6, 2012; and

WHEREAS, pursuant to the adoption of Ordinance No. 36 ORD 12-12, which was entitled "AN ORDINANCE DEFINING THE BOUNDARY OF THE EXTRATERRITORIAL PLANNING JURISDICTION OF THE CITY OF ASHEBORO," the extraterritorial planning jurisdiction boundary line shown on a map entitled "City of Asheboro 2012 Revised Extra-Territorial Planning Jurisdiction Area," which consisted of seven (7) sheets collectively prepared under Job No. 12030 by Thomas Scaramastra, Professional Land Surveyor with License No. L-4221, was approved and adopted, effective January 1, 2013, as the official boundary line for the extraterritorial planning jurisdiction of the City of Asheboro; and

WHEREAS, the said map entitled "City of Asheboro 2012 Revised Extra-Territorial Planning Jurisdiction Area" has been recorded in the Office of the Randolph County Register of Deeds in Plat Book 135, Pages 28-34; and

WHEREAS, by virtue of the adoption of this revised extraterritorial planning jurisdiction boundary line with an effective date of January 1, 2013, a substantial amount of territory previously located within the City of Asheboro's extraterritorial planning jurisdiction was released from the city's jurisdiction; and

WHEREAS, under North Carolina law, members of a municipality's Planning Board who represent the extraterritorial area only act on matters affecting that area unless the city's ordinance specifically grants these members equal authority on matters within the city limits; and

WHEREAS, similar to the majority of other cities in North Carolina, the City of Asheboro has explicitly authorized all members of the Planning Board, including the representatives of the extraterritorial area, to vote on matters that come before the Board regardless of whether the issue under consideration pertains to an area within or beyond the city limits; and

WHEREAS, the fact that all members of the Planning Board vote on any matter that comes before the Board creates a need to monitor the composition of the Board to determine if the number of extraterritorial members on the Planning Board is proportional to the population of the extraterritorial area relative to the city's population; and

WHEREAS, the above-described relinquishment of a substantial amount of territory from the city's jurisdiction has called into question the size and composition of the city's Planning Board; and

WHEREAS, the City Council has concluded that the size of the Planning Board should be reduced and the ratio of Board members who are city residents compared to the number of members from beyond the city limits should be adjusted to reflect the significantly reduced size of the population residing within the city's extraterritorial planning jurisdiction; and

WHEREAS, during a regular meeting of the Planning Board on October 7, 2013, and as reflected in the approved minutes of the said meeting, the Planning Board members who do not currently reside in an area subject to the city's planning jurisdiction have continued their tradition of assisting the City Council in its on-going efforts to update the city's framework for regulating land use and development by submitting their resignations, effective at the end of the day on December 31, 2013, in order to facilitate the Council's modification of the Planning Board's size and composition to reflect the significant reduction, that became effective on January 1, 2013, of the city's extraterritorial planning jurisdiction; and

WHEREAS, in preparation for considering the adoption of an ordinance that would implement the contemplated changes in the size and composition of the Planning Board by amending Chapter 34 (Departments, Boards and Commissions) of the Code of Asheboro, a public hearing on the proposed legislative action was scheduled for the City Council's regular November meeting; and

WHEREAS, after notice of this public hearing was published in *The Courier-Tribune* on October 24, 2013, and on October 31, 2013, the advertised public hearing was held on November 7, 2013, and the information presented during the hearing was considered prior to the adoption of this Ordinance;

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Asheboro, North Carolina as follows:

Section 1. Section 34.51 (titled "Creation; Composition and Membership") of the Code of Asheboro is hereby rewritten to provide as follows:

(A) Pursuant to Article 19 of Chapter 160A of the General Statutes of North Carolina, there is hereby created a Planning Board, which shall consist of ~~ten~~ **seven** members, five of whom shall be residents of the city and ~~five~~ **two** of whom shall be residents of the extraterritorial area ~~as is~~ shown on the map entitled "City of Asheboro 2012 Revised Extra-Territorial Planning Jurisdiction Area" and recorded in the Office of the Randolph County Register of Deeds in Plat Book 135, Pages 28-34. ~~Notwithstanding the preceding residency requirements, any member of the Planning Board who, as of January 10, 2013, is serving on the board as a representative of the extraterritorial area and no longer resides in the said area may nonetheless serve the remainder of his or her current term of office, and no more, if the sole reason for the board member's noncompliance with the residency requirement is the City Council's adoption of the new boundary line for the extraterritorial area that became effective on January 1, 2013.~~

(B) All members of the Planning Board shall have equal rights, privileges and duties with the other members in all matters, wherever they might arise.

(C) The Planning Board may exercise the powers granted by this Code and other ordinances and by G.S. Ch. 160A, Art. 19, within the city and within the extraterritorial area ~~as is~~ shown on the map entitled "City of Asheboro 2012 Revised Extra-Territorial Planning Jurisdiction Area" and recorded in the Office of the Randolph County Register of Deeds in Plat Book 135, Pages 28-34.

Section 2. Section 34.52 (titled "Appointment and Term; Vacancies; Attendance; Compensation") of the Code of Asheboro is hereby rewritten to provide as follows:

~~(A) Members of the Planning Board shall be appointed for a term of five years and until their successors are appointed and qualify, and, to provide overlapping terms, two members shall be appointed each year during the month of January. Of the two annual appointees, one shall be a resident of the city and one shall be a resident of the extraterritorial area as is shown on the map entitled "City of Asheboro 2012 Revised Extra-Territorial Planning Jurisdiction Area" and recorded in the Office of the Randolph County Register of Deeds in Plat Book 135, Pages 28-34. Notwithstanding the preceding residency requirements, any member of the Planning Board who, as of January 10, 2013, is serving on the board as a representative of the extraterritorial area and no longer resides in the said area may nonetheless serve the remainder of his or her current term of office, and no more, if the sole reason for the board member's noncompliance with the residency requirement is the City Council's adoption of the new boundary line for the extraterritorial area that became effective on January 1, 2013.~~

(A) Members of the Planning Board shall be appointed for a term of five years and until their successors are appointed and qualify. The appointment of Planning Board members shall be conducted as follows:

(1) One city resident shall be appointed by the City Council in January of each year to a five-year term of office. This five-year term of office shall be deemed to commence on the first day of February during the year in which the appointment to the full term of office is made.

(2) One Planning Board member appointed by the Randolph County Board of Commissioners to provide representation for the city's extraterritorial planning jurisdiction area shall serve a five-year term of office that expires in 2018. This seat on the Planning Board shall initially be held by the Planning Board member appointed by the Randolph County Board of Commissioners to a five-year term of office in February 2013. Hereafter, the Randolph County Board of Commissioners shall continue to make appointments to this office on a schedule that will require the next appointment to a full term of office to occur in January 2018, and every five years thereafter. The commencement date for these five-year terms of office shall be deemed to be the first day of February during the year in which the appointment to the full term of office is made.

(3) The other member of the Planning Board to be appointed by the Randolph County Board of Commissioners to provide representation for the city's extraterritorial planning jurisdiction area shall serve a five-year term of office that will commence on February 1, 2014. The initial appointment to this office is to occur in January 2014, and successive appointments shall occur every five years thereafter.

(B) Members of the Planning Board who are residents of the city shall be appointed by the City Council. Members of the Planning Board who are residents of the extraterritorial area ~~as is~~ shown on the map entitled "City of Asheboro 2012 Revised Extra-Territorial Planning Jurisdiction Area" and recorded in the Office of the Randolph County Register of Deeds in Plat Book 135, Pages 28-34 shall be appointed by the Randolph County Board of Commissioners; provided, that in the event such Board of County Commissioners fails to make any appointment provided for in this section within 90 days after the receipt of a resolution from the City Council requesting that such appointment be made, the City Council may thereupon make such appointment.

(C) Vacancies occurring for reasons other than expiration of terms shall be filled as they occur for the period of the unexpired term.

(D) Faithful attendance at the meetings of the Planning Board is considered a prerequisite for the maintenance of membership on the Board. A Planning Board member who is absent from three of any four consecutive regular meetings is subject to removal from office by the governing board that appointed the individual. When the removal of a Planning Board member for noncompliance with attendance requirements is under consideration, notice of the proposed action and an opportunity to be heard by the appointing authority on the question of the proposed removal from office shall be provided to the Board member facing the possibility of removal from office. Delivery of such notice of the opportunity to be heard shall be deemed to be sufficient if written notice of the opportunity is sent, at least fifteen calendar days in advance of the meeting during which removal is to be considered, via first class United States mail to the most recent address provided to the city by the member subject to removal.

(E) On and after April 1, 2009, each member of the Planning Board shall receive a stipend of \$100 for each month that the member actively serves on the Planning Board. In order to be deemed to be actively serving on the Planning Board during any specific month, a member of the Board must attend, at a minimum, the regular Planning Board meeting scheduled for the month for which the monthly stipend is to be paid. The actual disbursement of the monthly stipends for Planning Board members shall be made according to a schedule to be developed by the City of Asheboro Finance Department in accordance with the Department's normal business practices. Irrespective of the number of special meetings that may be held during any specific month, the amount of the monthly stipend for Planning Board members shall not exceed \$100.

Section 3. Section 34.53 (titled "Organization and Meetings") of the Code of Asheboro is hereby rewritten to provide as follows:

Each year, within 30 days after the annual appointment of new members, the Planning Board shall meet and elect a chairman and create and fill such offices as it may determine. The term of the chairman and other officers shall be one year, with eligibility for reelection. The Planning Board shall adopt rules for transaction of its business and governing its resolutions, discussions, findings and recommendations, and such rules shall be a public record. The Board shall hold at least one meeting monthly, and all of its meetings shall be open to the public. A majority of the Planning Board's membership shall constitute a quorum. When determining whether or not a quorum is present, vacant positions on the Board and Planning Board members who are disqualified from voting because of a statutorily recognized conflict of interest shall not be considered members of the Board for the limited purpose of calculating how many Board members are needed for the requisite majority. ~~There shall be a quorum of six members for the purpose of taking any official action required by this Code.~~

Section 4. All ordinances and clauses of ordinances in conflict with this ordinance are hereby repealed.

Section 5. This ordinance shall take effect and be in force from and after January 1, 2014.

This ordinance was adopted in open session during a regular meeting of the Asheboro City Council that was held on November 7, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

(d) Consideration of a request to extend the time allowed between the Preliminary and Final Plat subdivision reviews for Waterford Villas.

Mr. Nuttall presented a written request from Mr. Kevin Jessup, Manager of Waterford RE, LLC for a twelve month extension of the deadline to submit a final subdivision plat for review. This delay is requested because of the overall building market conditions and financing of the project. The developers expect to continue the project as market conditions improve.

Upon motion by Mr. Bell and seconded by Mr. Carter, Council voted unanimously to approve the requested extension.

- (e) **Discussion of potential transportation projects (excluding aviation) for consideration by the Piedmont Triad Rural Planning Organization as part of development of North Carolina Department of Transportation's FY 2016-2020 Transportation Improvement Program and FY 2021-2025 Developmental Plan.**

Mr. Nuttall briefly presented a list of potential transportation projects that included highway, pedestrian, and bicycle plans that may be considered by the Piedmont Triad Rural Planning Organization and could be included as part of the development of the North Carolina Department of Transportation's improvement and development plans.

Supportive comments were offered by the elected officials. No action was requested of the governing board, and none was taken on this matter.

7. Public comment period.

Mayor Smith opened the floor for comments from the public.

There being no comments from the public, Mayor Smith closed the public comment period.

8. Annexation items:

- (a) **Consideration of a petition received from Clapp Rental Property Family Limited Partnership requesting contiguous annexation of 0.22 of an acre of land at 1346 East Presnell Street.**

(i) **Public hearing**

Mayor Smith opened the public hearing on the request (petition) for contiguous annexation of 0.22 of an acre of land located 1346 East Presnell Street.

There were no comments and no opposition from the public. Mayor Smith then transitioned to the deliberative phase of the public hearing.

(ii) **Ordinance to extend the corporate limits of the City of Asheboro**

Mr. Leonard presented and recommended adoption, by reference, of an ordinance to extend the corporate limits of the City of Asheboro.

Upon motion by Mr. Baker and seconded by Mr. Burks, Council voted unanimously to adopt the following ordinance by reference.

Ordinance Number 32 ORD 11-13
ORDINANCE TO EXTEND THE CORPORATE LIMITS OF THE
CITY OF ASHEBORO, NORTH CAROLINA
(Annexed Area = Parcel of Land Located at 1346 East Presnell Street)

WHEREAS, pursuant to Section 160A-31 of the North Carolina General Statutes, a petition requesting annexation by the City of Asheboro of the territory described in Section 1 of this Ordinance was presented to the Asheboro City Council on October 10, 2013; and

WHEREAS, this petition for annexation was properly executed by an authorized individual on behalf of the owner of the parcel of land for which annexation was requested; and

WHEREAS, by means of a duly adopted resolution (Resolution Number 43 RES 10-13), the Asheboro City Council directed the City Clerk to investigate the sufficiency of the petition, and the Clerk has certified the sufficiency of the said petition; and

WHEREAS, pursuant to Asheboro City Council Resolution Number 44 RES 10-13, notice was published to the public on the 24th day of October, 2013, in *The Courier-Tribune*, a newspaper having general circulation in the City of Asheboro, setting forth that a public hearing would be held during the Asheboro City Council's next regular meeting that would begin at 7:00 o'clock p.m. on the 7th day of November, 2013, in the Council Chamber at Asheboro City Hall, to consider the adoption of an ordinance annexing the said area into the City of Asheboro; and

WHEREAS, the said public hearing was in fact held on the 7th day of November, 2013; and

WHEREAS, the Asheboro City Council has determined that the petition meets the requirements of Section 160A-31 of the North Carolina General Statutes;

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Asheboro, North Carolina as follows:

Section 1. By virtue of the authority granted in Section 160A-31 of the North Carolina General Statutes, the following described area is hereby annexed and made part of the City of Asheboro, North Carolina:
Asheboro Township, Randolph County, North Carolina:

BEGINNING on the existing primary city limits line for the City of Asheboro at $\frac{3}{8}$ " existing iron rod set 6" down in the southern margin of the public right-of-way for East Presnell Street (North Carolina Secondary Road 2345) at the northeast corner of the parcel of land for which annexation has been requested (the Clapp Rental Property Family Limited Partnership property that is described in Deed Book 1640, Page 688, Randolph County Public Registry and which will be hereinafter referred to as the "Annexation Parcel"), the said beginning point is North 88 degrees 21 minutes 49 seconds West 90.29 feet from a right-of-way monument that is flush with the ground in the southern margin of the public right-of-way for East Presnell Street and is located by means of the North Carolina Coordinate System at the coordinates of North 715,974.05 feet and East 1,766,587.20 feet (NAD 83); thence departing from the existing city limits line and the said beginning point by proceeding along the proposed city limits line, which follows the boundary lines of the Annexation Parcel, the following courses and distances: South 02 degrees 28 minutes 41 seconds West 123.66 feet along the eastern boundary line of the Annexation Parcel to a $\frac{1}{2}$ " existing iron rod that is up 6"; thence South 89 degrees 37 minutes 11 seconds West 76.19 feet along the southern boundary line of the Annexation Parcel to a 1" existing iron pipe that is up 10"; thence along the Annexation Parcel's western boundary line the following course and distance: North 02 degrees 56 minutes 10 seconds East 126.19 feet to a right-of-way monument that is flush with the ground in the southern margin of the public right-of-way for East Presnell Street; thence along the southern margin of the public right-of-way for East Presnell Street, which is also the existing primary city limits line for the City of Asheboro, the following course and distance: South 88 degrees 29 minutes 33 seconds East 75.09 feet to the point and place of BEGINNING, and containing 9,441 square feet (or 0.22 of an acre) of land, more or less, to be annexed.

The above-listed description is in accordance with a plat of survey entitled "ANNEXATION PLAT FOR CLAPP RENTAL PROPERTY FAMILY LIMITED PARTNERSHIP" that was drawn under the supervision of Charles E. Morgan, Jr., Professional Land Surveyor with Registration Number L-3945, from an actual survey made under his supervision. The said plat of survey is dated August 22, 2013, and the following job number has been listed on the plat: 6744 C 10.

Section 2. Upon and after November 7, 2013, the above-described territory and its citizens and property shall be subject to all debts, laws, ordinances, and regulations in force in the City of Asheboro and shall be entitled to the same privileges and benefits as other parts of the City of Asheboro. Said territory shall be subject to municipal taxes according to Section 160A-58.10 of the North Carolina General Statutes.

Section 3. The Mayor of the City of Asheboro shall cause to be recorded in the office of the Register of Deeds of Randolph County, North Carolina, and in the office of the Secretary of State of North Carolina at Raleigh, North Carolina, an accurate map of the annexed territory, described in Section 1 above, together with a duly certified copy of this ordinance. Such a map shall also be delivered to the Randolph County Board of Elections, as required by Section 163-288.1 of the North Carolina General Statutes.

Section 4. All ordinances and clauses of ordinances in conflict with this ordinance are hereby repealed.

Section 5. This ordinance shall be in full force and effect upon and after the 7th day of November, 2013.

This ordinance was adopted by the Asheboro City Council in open session during a regular meeting held on the 7th day of November, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

Approved as to form:

/s/Jeffrey C. Sugg
Jeffrey C. Sugg, City Attorney
City of Asheboro, North Carolina

- (b) **Consideration of a petition received from Habitat for Humanity of Randolph County, N.C., Inc. requesting contiguous annexation of 0.38 of an acre of land located on the west side of North McCrary Street, approximately 400 feet south of the intersection of North McCrary Street and Wilson Street.**

(i) **Public hearing**

Mayor Smith opened the public hearing on the request (petition) for contiguous annexation of 0.38 of an acre of land located on the west side of North McCrary Street, approximately 400 feet south of the intersection of North McCrary Street and Wilson Street.

There were no comments and no opposition from the public. Mayor Smith then transitioned to the deliberative phase of the public hearing.

(ii) **Ordinance to extend the corporate limits of the City of Asheboro**

Mr. Leonard presented and recommended adoption, by reference, of an ordinance to extend the corporate limits of the City of Asheboro.

Upon motion by Mr. Swiers and seconded by Mr. Bell, Council voted unanimously to adopt the following ordinance by reference.

Ordinance Number 33 ORD 11-13
ORDINANCE TO EXTEND THE CORPORATE LIMITS OF THE
CITY OF ASHEBORO, NORTH CAROLINA
(Annexed Area = 0.44 of an Acre of Land on the West Side of North McCrary Street)

WHEREAS, pursuant to Section 160A-31 of the North Carolina General Statutes, a petition requesting annexation by the City of Asheboro of the territory described in Section 1 of this Ordinance was presented to the Asheboro City Council on October 10, 2013; and

WHEREAS, this petition for annexation was properly executed by the authorized officers of the non-profit corporation that owns the parcel of land for which annexation was requested; and

WHEREAS, by means of a duly adopted resolution (Resolution Number 45 RES 10-13), the Asheboro City Council directed the City Clerk to investigate the sufficiency of the petition, and the Clerk has certified the sufficiency of the said petition; and

WHEREAS, pursuant to Asheboro City Council Resolution Number 46 RES 10-13, notice was published to the public on the 24th day of October, 2013, in *The Courier-Tribune*, a newspaper having general circulation in the City of Asheboro, setting forth that a public hearing would be held during the Asheboro City Council's next regular meeting that would begin at 7:00 o'clock p.m. on the 7th day of November, 2013, in the Council Chamber at Asheboro City Hall, to consider the adoption of an ordinance annexing the said area into the City of Asheboro; and

WHEREAS, the said public hearing was in fact held on the 7th day of November, 2013; and

WHEREAS, the Asheboro City Council has determined that the petition meets the requirements of Section 160A-31 of the North Carolina General Statutes;

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Asheboro, North Carolina as follows:

Section 1. By virtue of the authority granted in Section 160A-31 of the North Carolina General Statutes, the following described area is hereby annexed and made part of the City of Asheboro, North Carolina:

Asheboro Township, Randolph County, North Carolina:

BEGINNING at a ½" existing iron rod that is 2" above the ground at the northeast corner of the parcel of land for which annexation has been requested (the Habitat for Humanity of Randolph County, N.C., Inc. property that is described in Deed Book 2345, Page 1167, Randolph County Public Registry and which will be hereinafter referred to as the "Annexation Parcel"), the said beginning point is on the western margin of the 30-foot public right-of-way for North McCrary Street and is located the following courses and distances from a 1-¼" existing iron pipe that is flush with the ground and is located by means of the North Carolina Coordinate System at the coordinates of North 727,295.75 feet and East 1,754,076.16 feet (NAD 27): South 05 degrees 46 minutes 47 seconds West 95.62 feet to a bent 1" existing iron pipe; thence South 05 degrees 30 minutes 31 seconds West 95.70 feet to the said beginning point; thence from the said beginning point along the proposed city limits line, which follows the western margin of the 30-foot public right-of-way for North McCrary Street, South 05 degrees 59 minutes 14 seconds West

95.59 feet to a 1" existing iron rod that is 2" down at the southeast corner of the Annexation Parcel; thence proceeding along the southern boundary line of the Annexation Parcel the following course and distance: North 85 degrees 44 minutes 57 seconds West 199.59 feet to $\frac{5}{8}$ " existing iron rod that is up 24" at the southwest corner of the Annexation Parcel; thence departing from the proposed city limits line and proceeding along the existing primary city limits line for the City of Asheboro, which follows the western boundary line of the Annexation Parcel, the following course and distance: North 05 degrees 50 minutes 00 seconds East 95.70 feet to a 1- $\frac{1}{4}$ " existing iron pipe that is up 3" at the northwest corner of the Annexation Parcel; thence departing from the existing primary city limits line and following the proposed city limits line South 85 degrees 42 minutes 58 seconds East 199.84 feet along the northern boundary line of the Annexation Parcel to the point and place of BEGINNING, and containing 19,094 square feet (or 0.44 of an acre) of land, more or less, to be annexed.

The above-listed description is in accordance with a plat of survey entitled "ANNEXATION PLAT FOR HABITAT FOR HUMANITY OF RANDOLPH COUNTY NC INC." that was drawn under the supervision of Charles E. Morgan, Jr., Professional Land Surveyor with Registration Number L-3945, from an actual survey made under his supervision. The said plat of survey is dated September 23, 2013, and the following job number has been listed on the plat: 6767 H 10.

Section 2. Upon and after November 7, 2013, the above-described territory and its citizens and property shall be subject to all debts, laws, ordinances, and regulations in force in the City of Asheboro and shall be entitled to the same privileges and benefits as other parts of the City of Asheboro. Said territory shall be subject to municipal taxes according to Section 160A-58.10 of the North Carolina General Statutes.

Section 3. The Mayor of the City of Asheboro shall cause to be recorded in the office of the Register of Deeds of Randolph County, North Carolina, and in the office of the Secretary of State of North Carolina at Raleigh, North Carolina, an accurate map of the annexed territory, described in Section 1 above, together with a duly certified copy of this ordinance. Such a map shall also be delivered to the Randolph County Board of Elections, as required by Section 163-288.1 of the North Carolina General Statutes.

Section 4. All ordinances and clauses of ordinances in conflict with this ordinance are hereby repealed.

Section 5. This ordinance shall be in full force and effect upon and after the 7th day of November, 2013.

This ordinance was adopted by the Asheboro City Council in open session during a regular meeting held on the 7th day of November, 2013.

/s/David H. Smith
David H. Smith, Mayor
City of Asheboro, North Carolina

ATTEST:

/s/Holly H. Doerr
Holly H. Doerr, CMC, NCCMC, City Clerk
City of Asheboro, North Carolina

Approved as to form:

/s/Jeffrey C. Sugg
Jeffrey C. Sugg, City Attorney
City of Asheboro, North Carolina

9. Upcoming events:

- **Housing Summit sponsored by the Randolph County Housing Coalition, Inc. on November 8, 2013 from 8:00 a.m. until 2:00 p.m. at Randolph-Asheboro YMCA.**
- **Annual Christmas Parade begins at 7:00 p.m. on December 6, 2013.**
- **"Christmas on Sunset" street festival begins at 6:00 p.m. on December 13, 2013.**

There being no further business, the meeting was adjourned at 8:05 p.m.

Holly H. Doerr, CMC, NCCMC, City Clerk

David H. Smith, Mayor

Minutes of the meeting of the Asheboro Alcoholic Beverage Control Board held on October 7, 2013

The Asheboro ABC Board met on October 7, 2013, at 5:30 PM, in the Board office, 700 South Fayetteville Street, Asheboro, NC.

Present were Chair Brooke Schmidly and Board Member Steve Knight and General Manager Rodney Johnson (GM). A quorum being present, the Chair called the meeting to order for the transaction of business and business transacted as follows:

The Chair inquired as to any known conflict of interest, appearance of a conflict of interest, or objections concerning agenda items before the Board; after the Chair and both Board members voiced having no conflict, and there being no objection, the agenda was adopted.

The Board reviewed and there being no objection, approved the Minutes from the September 3, 2013, regular meeting.

The Board is required to undergo an annual independent audit of its operations (GS 18B-702(s)) and submit a copy of the completed audit to the City of Asheboro and the ABC Commission: the required independent audit for fiscal year ending June 30, 2013, prepared by Lewis & Lewis CPAs, was received by the Board. The GM reported a copy of the audit was submitted by Lewis & Lewis to the ABC Commission and a copy will be submitted to the City of Asheboro with the meeting minutes.

Steve Knight presented an overview of the audit reporting the Board collected \$2,896,748.00 from sales and \$670,736.00 from tax collections (\$3,567,484.00 total collections). Gross alcohol sales increased 6% over the prior year. The Board distributed \$11,557.00 to the City of Asheboro for law enforcement; \$16,179.00 to local organizations for the treatment of alcoholism or substance abuse or for research or education on alcohol or substance abuse; and \$230,000 as a final profit distribution to the City of Asheboro. An overall financial summary indicates:

- The Board's net financial position increased by 13%
- Income from operations increased by 9%
- Capital assets increased by 8.25%.

A copy of the "Audited Financial Statements June 30, 2013 and 2012" is attached hereto and incorporated herein by reference.

Following unfinished business from the September 3, 2013, meeting, the Chair moved all grant award applications received by the Board by October 7, 2013, be considered. The motion was approved by the Board.

The Board reviewed grant requests for Alcohol Education and Rehabilitation funds from for 3rd and 4th Quarter 2013 funds. After discussion, the Chair moved and the Board approved that available funds be disbursed as follows:

- \$585 to 4-H Youth Development, NC Cooperation Extension, Randolph County Center, from 3rd Quarter funds.
- \$4,800 to Randolph Fellowship Homes, from available 3rd and 4th Quarter funds.
- \$3,084 to Randolph Fellowship Homes, from remaining 3rd and 4th Quarter funds.

The Board reviewed and discussed the 3rd Quarter Violation log summarizing actions against retail Asheboro ABC permittees by the NC ABC Commission.

At the Board's July 1, 2013, Board Meeting, the Board approved funding for Fiscal Year 2013-2014 employee merit increases. The GM becomes eligible for a merit increase effective October 15. After discussion, Steve Knight moved to fund the GM's merit increase and the motion was approved by the Board.

The Board heard reports from the General Manager concerning the following issues:

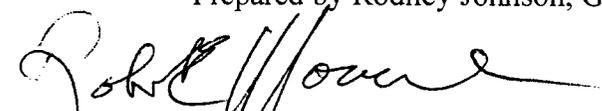
1. The Change-Fund Report ending September 30, 2013, was reviewed with nothing remarkable noted.
2. Asheboro ABC sales statistics comparing:
 - September 2013 sales with the previous month indicate:
 - An overall -14.4% change (all sales and tax collections)
 - September 2013 sales with sales from the same month last year indicate:
 - Retail Sales: -1.4%
 - Mixed Beverage Sales: +15.6%
 - Sales Tax Collections: -1.3%
 - Overall Collections: +0.3%
 - September 2013 bottle sales with bottle sales from the same month last year indicate:
 - Retail Bottle Sales: +1.0%
 - Mixed Beverage Bottle Sales: +9.1%
 - Overall Bottle Sales: +1.5%

Charts reflecting sales histories were handed out to Board members for review and discussion.

The next regular Asheboro ABC Board meeting will be held Monday, November 4, 2013.

There being no further business, the meeting was adjourned.

Prepared by Rodney Johnson, GM, and Approved by the Board 11-4-2013 
GM




**ASHEBORO BOARD
OF ALCOHOLIC BEVERAGE CONTROL
ASHEBORO, NORTH CAROLINA**

AUDITED FINANCIAL STATEMENTS

JUNE 30, 2013 and 2012

Board of Directors

Chairman
Brooke Schmidly

Board Member
Stephen Knight

Board Member
Robert Morrison

Finance Officer
Rodney Johnson

Prepared By
**LEWIS & LEWIS CPAs PLLC
LUMBERTON, NORTH CAROLINA
ROCKINGHAM, NORTH CAROLINA**

Asheboro Board of Alcoholic Beverage Control

(A component unit of The City of Asheboro)

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Lewis & Lewis CPAs, PLLC

Henry G. Lewis, CPA • Durham E. Lewis, CPA • Henry "Hank" Lewis, Jr., CPA
Member AICPA • Member NCACPA

Independent Auditor's Report

Board of Directors
Asheboro Board of Alcoholic Beverage Control
Asheboro, NC

Report on the Financial Statements

We have audited the accompanying financial statements of the Asheboro Board of Alcoholic Beverage Control, a component unit of the City of Asheboro, which comprise the Statement of Net Position as of June 30, 2013 and 2012, and the related Statement of Revenues, Expenses and Changes in Net Position, and Statement of Cash Flows for the year then ended and the related notes to the financial statements.

Management's responsibility for the Financial Statements

Management is responsible for the preparation and fair presentation of these financial statements in accordance with accounting principles generally accepted in the United States of America; this includes the design, implementation, and maintenance of internal control relevant to the preparation and fair presentation of financial statements that are free from material misstatement, whether due to fraud or error.

Auditor's Responsibility

Our responsibility is to express an opinion on these financial statements based on our audits. We conducted our audits in accordance with auditing standards generally accepted in the United States of America. Those standards require that we plan and perform the audits to obtain reasonable assurance about whether the financial statements are free of material misstatement.

An audit involves performing procedures to obtain evidence about the amounts and disclosures in the financial statements. The procedures selected depend on the auditor's judgment, including the assessment of the risks of material misstatement of the financial statements, whether due to fraud or error. In making those risk assessments, the auditor considers internal control relevant to the entity's preparation and fair presentation of the financial statements in order to design audit procedures that are appropriate in the circumstances, but not for the purpose of expressing an opinion on the effectiveness of the entity's internal control. Accordingly, we express no such opinion. An audit also includes evaluating the appropriateness of accounting policies used and the reasonableness of significant accounting estimates made by management, as well as evaluating the overall presentation of the financial statements.

We believe that the audit evidence we have obtained is sufficient and appropriate to provide a basis for our audit opinions.

Opinions

In our opinion, the financial statements referred to above present fairly, in all material respects, the financial position of the Asheboro Board of Alcoholic Beverage Control as of June 30, 2013 and 2012, and the respective changes in financial position, and where applicable, cash flows thereof for the years ended in accordance with accounting principles generally accepted in the United States of America.

Other Matters

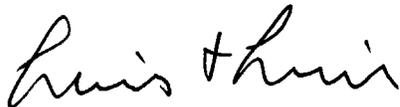
Required Supplementary Information

Accounting principles generally accepted in the United States of America require that the Management's Discussion and Analysis on pages 3, 4, and 5 be presented to supplement the basic financial statements. Such information, although not a part of the basic financial statements, is required by the Governmental Accounting Standards Board, who considers it to be an essential part of financial reporting for placing the basic financial statements in an appropriate operational, economic, or historical context. We have applied certain limited procedures to the required supplementary information in accordance with auditing standards generally accepted in the United States of America, which consisted principally of inquiries of management about the methods of preparing the information and comparing the information for consistency with management's responses to our inquiries, the basic financial statements, and other knowledge we obtained during our audit of the basic financial statements. We do not express an opinion or provide any assurance on the information because the limited procedures do not provide us with sufficient evidence to express an opinion or provide any assurance.

Other Information

Our audits were conducted for the purpose of forming opinions on the financial statements as a whole. The Schedule of Store Expenses, Administrative Expenses, and Schedule of Revenues and Expenditures – Budget and Actual are presented for purposes of additional analysis and are not a required part of the basic financial statements.

The Schedule of Expenses by Store, Administrative Expenses, and Schedule of Revenues and Expenditures – Budget and Actual are the responsibility of management and were derived from and related directly to the underlying accounting and other records used to prepare the financial statements. The information has been subjected to the auditing procedures applied in the audit of the financial statements and certain additional procedures applied in the audit of the financial statements and certain additional procedures, including comparing and reconciling such information directly to the underlying accounting and other records used to prepare the basic financial statements themselves, and other additional procedures in accordance with auditing standards generally accepted in the United States of America. In our opinion, the information is fairly stated, in all material respects, in relation to the financial statements taken as a whole.



Lewis & Lewis CPAs PLLC
September 13, 2013

Management's Discussion and Analysis

This section of the Asheboro Board of Alcoholic Beverage Control (*the Board*) financial report represents our discussion and analysis of the financial performance of the Board for the year ended June 30, 2013. This information should be read in conjunction with the audited financial statements included in this report.

Financial Highlights

- Working capital was approximately \$401,682
- A total of \$230,000 voluntary distributions were made by the board to the City of Asheboro during the year.

Overview of the Financial Statements

The audited financial statements of the ABC Board consist of 3 components. They are as follows:

- *Management's Discussion and Analysis*
- *Basic Financial Statements*
- *Additional Information Required by the ABC Commission*

The *Basic Financial Statements* are prepared using the full accrual basis of accounting. They consist of three statements. The first statement is the **Statement of Net Position**. Assets and liabilities are classified between current and long-term. This statement provides a summary of the Board's investment in assets and obligations to creditors. Liquidity and financial flexibility can be evaluated using the information contained in this statement.

The next statement is the **Statement of Revenues, Expenses, and Changes in Net Position**. This statement is used in evaluating whether the Board has recovered all of its costs through sales. Its information is used in determining credit worthiness.

The final required statement is the **Statement of Cash Flows**. This statement reports cash inflows and outflows in the following categories: operating, investing, and financing activities. Based on this data, the user can determine the sources of cash, the uses of cash, and the change in cash.

The notes to the financial statements provide more detailed information and should be read in conjunction with the statements.

The ABC Commission requires some schedules in addition to the information required by generally accepted accounting principles. They include a **Schedule of Store Expenses**, a **Schedule of Administrative Expenses**, **Schedule of Warehouse Expenses** and a **Schedule of Revenues and Expenditures – Budget and Actual**.

Financial Analysis of the ABC Board

Net position is an indicator of the fiscal health of the Board. Assets exceeded liabilities by \$508,881 in 2013. The largest components of net assets were cash and inventory. Together they totaled 95% of the total net assets. Following is a summary of the Statement of Net Assets:

Table 1
Condensed Statements of Net Position

	<u>2013</u>	<u>2012</u>	<u>2011</u>	<u>\$ Change</u>	<u>% Change</u>
Current assets	\$ 493,042	\$ 429,977	\$ 371,021	\$ 63,065	15%
Non-current assets	97,426	105,854	120,771	(8,428)	-8%
Total assets	<u>\$ 590,468</u>	<u>\$ 535,831</u>	<u>\$ 491,792</u>	<u>\$ 54,637</u>	<u>10%</u>
Current liabilities	\$ 81,587	\$ 83,792	\$ 86,740	\$ (2,205)	-3%
Non-current liabilities	-	-	-	-	0%
Total liabilities	<u>\$ 81,587</u>	<u>\$ 83,792</u>	<u>\$ 86,740</u>	<u>\$ (2,205)</u>	<u>-3%</u>
Net invested in capital assets	\$ 97,316	\$ 105,744	\$ 120,771	\$ (8,428)	-8%
Restricted net position	85,202	82,017	77,408	3,185	4%
Unrestricted net position	326,363	264,278	206,873	62,085	23%
Total net position	<u>\$ 508,881</u>	<u>\$ 452,039</u>	<u>\$ 405,052</u>	<u>\$ 56,842</u>	<u>13%</u>

Net position increased by 13% from the prior year. Income from operations increased by 9% from the prior year
Following is a summary of the changes in net position:

Table 2
Condensed Statements of Revenues, Expenses, and Changes in Net Position

	<u>2013</u>	<u>2012</u>	<u>2011</u>	<u>\$ Change</u>	<u>% Change</u>
Operating revenues	\$ 2,896,748	\$ 2,777,754	\$ 2,617,592	\$ 118,994	4%
Less: Taxes on gross sales	670,736	645,275	605,005	25,461	4%
Net sales	\$ 2,226,012	\$ 2,132,479	\$ 2,012,587	\$ 93,533	4%
Cost of sales	1,504,972	1,452,396	1,367,275	52,576	4%
Gross profit	\$ 721,040	\$ 680,083	\$ 645,312	\$ 40,957	6%
Less: Operating expenses	406,732	388,308	380,094	18,424	5%
Income from operations	\$ 314,308	\$ 291,775	\$ 265,218	\$ 26,557	9%
Non-operating revenues and expenses	270	277	331	(7)	-3%
Change in net assets before distributions	\$ 314,578	\$ 292,052	\$ 265,549	\$ 22,526	8%
Distributions	257,736	245,065	127,509	12,671	5%
Change in net position	\$ 56,842	\$ 46,987	\$ 138,040	\$ 9,855	21%
Net position, beginning	452,039	405,052	267,012	46,987	12%
Net position, ending	<u>\$ 508,881</u>	<u>\$ 452,039</u>	<u>\$ 405,052</u>	<u>\$ 46,987</u>	<u>10%</u>

Following is a breakdown of sales by source:

	<u>2013</u>	<u>2012</u>	<u>2011</u>	<u>\$ Change</u>	<u>% Change</u>
Retail Liquor Sales	\$ 2,555,788	\$ 2,420,331	\$ 2,312,420	\$ 107,911	5%
Mixed Beverage Sales	330,188	346,023	293,965	52,058	18%
Retail Wine Sales	10,772	11,400	11,207	193	2%
Total Sales	<u>\$ 2,896,748</u>	<u>\$ 2,777,754</u>	<u>\$ 2,617,592</u>	<u>\$ 160,162</u>	<u>6%</u>

Capital Asset and Debt Administration

Capital Assets

Capital assets increased by 8.25% from the prior year.

**Table 3
Summary of Changes in Capital Assets**

	<u>2013</u>	<u>2012</u>	<u>2011</u>	<u>\$ Change</u>	<u>% Change</u>
Leasehold Improvement	\$ 620	\$ 620	\$ 620	\$ -	
Equipment	188,908	174,518	167,965	14,390	8.25%
Total	<u>\$ 189,528</u>	<u>\$ 175,138</u>	<u>\$ 168,585</u>	<u>\$ 14,390</u>	<u>8.25%</u>

Economic Factors

An improving economy has resulted in a large increase in sales, income from operations, and net assets. The Board anticipates continued growth which should match or exceed the current year's financial position.

Requests for Information

This report is intended to provide a summary of the financial condition of the ABC Board. Questions or requests for additional information should be addressed to:

Rodney Johnson, Finance Officer
Asheboro Board of Alcoholic Beverage Control
700 S Fayetteville Street
Asheboro, NC 27203

Asheboro Board of Alcoholic Beverage Control
(A component unit of the City of Asheboro)
Statements of Net Position
as of June 30, 2013
and June 30, 2012

	2013	2012
ASSETS		
CURRENT ASSETS		
Cash	\$ 254,202	\$ 207,932
Inventories	229,067	212,989
Prepaid Expenses	9,773	9,056
Total Current Assets	\$ 493,042	\$ 429,977
NON-CURRENT ASSETS		
Property Plant and Equipment (net of accumulated depreciation)	\$ 97,316	\$ 105,744
Utility Deposits	110	110
Total Non-Current Assets	\$ 97,426	\$ 105,854
TOTAL ASSETS	\$ 590,468	\$ 535,831
LIABILITIES AND NET POSITION		
CURRENT LIABILITIES		
Accounts Payable	\$ 59,213	\$ 61,759
Payroll Taxes Payable	822	1,126
Accrued Liabilities	15,205	15,621
Distributions Payable	6,347	5,286
Total Current Liabilities	\$ 81,587	\$ 83,792
Total Liabilities	\$ 81,587	\$ 83,792
NET POSITION		
Net Investment in Capital Assets	\$ 97,316	\$ 105,774
Restricted for Working Capital	85,202	82,017
Unrestricted	326,363	264,248
Total Net Position	\$ 508,881	\$ 452,039
TOTAL LIABILITIES AND NET POSITION	\$ 590,468	\$ 535,831

The accompanying notes are an integral part of the financial statements.

Asheboro Board of Alcoholic Beverage Control
(A component unit of the City of Asheboro)
Statements of Revenues, Expenses and Changes in Net Position
For the Years Ended June 30, 2013
and June 30, 2012

	<u>2013</u>	<u>2012</u>
OPERATING REVENUE		
Liquor Sales - Regular	\$ 2,555,788	\$ 2,420,331
Mixed Beverage Sales	330,188	346,023
Wine Sales	10,772	11,400
Total Gross Sales	<u>\$ 2,896,748</u>	<u>\$ 2,777,754</u>
DEDUCT TAXES ON GROSS SALES		
State Excise Tax	\$ 625,568	\$ 598,447
Mixed Beverage Tax - NCDHHR	3,171	3,294
Mixed Beverage Tax - NCDOR	31,162	32,939
Rehabilitation Tax (Bottle)	10,051	9,773
Sales & Use Tax	784	822
Total Taxes	<u>\$ 670,736</u>	<u>\$ 645,275</u>
NET SALES	<u>\$ 2,226,012</u>	<u>\$ 2,132,479</u>
DEDUCT COST OF SALES		
Cost of Liquor Sold	\$ 1,496,980	\$ 1,444,485
Cost of Wine Sold	7,992	7,911
Total Cost of Sales	<u>1,504,972</u>	<u>1,452,396</u>
GROSS PROFIT ON SALES	<u>\$ 721,040</u>	<u>\$ 680,083</u>
DEDUCT OPERATING EXPENSES		
Store Expenses	\$ 321,220	\$ 304,124
Warehouse & Delivery Expenses	40,453	40,506
Administrative Expenses	22,240	22,100
Depreciation Expense	22,819	21,578
Total Operating Expenses	<u>\$ 406,732</u>	<u>\$ 388,308</u>
INCOME FROM OPERATIONS	<u>\$ 314,308</u>	<u>\$ 291,775</u>
Nonoperating Revenues (Expenses)		
Interest Income	\$ 270	\$ 277
CHANGE IN NET POSITION BEFORE DISTRIBUTION	<u>\$ 314,578</u>	<u>\$ 292,052</u>
DEDUCT DISTRIBUTIONS		
Law Enforcement	\$ 11,557	\$ 10,444
Alcohol Education & Rehab	16,179	14,621
Change in Net Position Before Profit Distributions	<u>\$ 286,842</u>	<u>\$ 266,987</u>
Profit Distributions - City of Asheboro	\$ 230,000	\$ 220,000
CHANGE IN NET POSITION	<u>\$ 56,842</u>	<u>\$ 46,987</u>
NET POSITION - JULY 01	<u>452,039</u>	<u>405,052</u>
NET POSITION - JUNE 30	<u>\$ 508,881</u>	<u>\$ 452,039</u>

The accompanying notes are an integral part of the financial statements.

Asheboro Board of Alcoholic Beverage Control
(A component unit of the City of Asheboro)
Statements of Cash Flows
For the Years Ended June 30, 2013
and June 30, 2012

	2013	2012
CASH FLOWS FROM OPERATING ACTIVITIES		
Cash received from customers	\$ 2,896,748	\$ 2,777,754
Cash payments to suppliers for goods & services	(1,521,050)	(1,452,922)
Cash payments to employees for services	(202,394)	(200,127)
Operating Expenses	(184,384)	(175,135)
Taxes paid	(671,932)	(638,835)
Net Cash Provided By Operating Activities	\$ 316,988	\$ 310,735
CASH FLOWS FROM CAPITAL AND RELATED FINANCING ACTIVITIES		
Acquisition of capital assets	\$ (14,390)	\$ (6,553)
Net Cash (Used) by Capital and Related Financing Activities	\$ (14,390)	\$ (6,553)
CASH FLOWS FROM NON-CAPITAL FINANCING ACTIVITIES		
Law Enforcement & Alcohol education distributions	\$ (26,599)	\$ (26,350)
Profit Distributions to Asheboro	(230,000)	(220,000)
Net Cash (Used) by Non-Capital Financing Activities	\$ (256,599)	\$ (246,350)
CASH FLOWS FROM INVESTING ACTIVITIES		
Interest Earned on Investments	\$ 271	\$ 277
NET (DECREASE) IN CASH	46,270	\$ 58,109
CASH - Beginning of Year	207,932	149,823
CASH - End of Year	\$ 254,202	\$ 207,932
Reconciliation of Income from Operations to Net Cash Provided by Operating Activities		
Net Income from Operations	\$ 314,308	\$ 291,775
Adjustments to reconcile income from operations to net cash provided by operating activities		
Depreciation	22,819	21,578
Changes in Assets and Liabilities		
(Increase) Decrease in Inventory	(16,078)	(526)
(Increase) Decrease in Prepaid Expenses	(718)	(429)
Increase (Decrease) in Liquor Taxes Payable	(1,732)	6,440
Increase (Decrease) in Accounts Payable	(1,731)	(1,684)
Increase (Decrease) in Payroll Taxes Payable	(304)	(3,514)
Increase (Decrease) in Accrued Liabilities	(416)	(2,905)
Increase (Decrease) in Liquor Sales Tax Payable	840	
Net Cash Provided by Operating Activities	\$ 316,988	\$ 310,735

The accompanying notes are an integral part of the financial statements.

Asheboro Board of Alcoholic Beverage Control
(A component unit of the City of Asheboro)
Notes to the Financial Statements
June 30, 2013

Note 1. Summary of Significant Accounting Policies

A. Principles used in determining the scope of the entity for financial reporting:

The Asheboro Board of Alcoholic Beverage Control (ABC Board), a component unit of the City of Asheboro, is a corporate body with powers outlined by General Statutes [Chapter 18B-701]. The governing body of the City of Asheboro appoints the members of the ABC Board.

The ABC Board is required by State Statute to distribute its surpluses to the general fund of Asheboro, which represents a benefit to the City. Therefore, the ABC Board is reported as a discretely presented component unit in the City's financial statements.

B. Organizational History

The Board was organized after a city wide election held July 29, 2008. The Asheboro City Council appointed three individuals to serve on the Board with terms of three years, two years and one year. The Asheboro ABC Store opened for business November 18, 2008.

C. Basis of Presentation

All activities of The Board are accounted for within a single proprietary (enterprise) fund. Proprietary funds are used to account for operations that are (a) financed and operated in a manner similar to private business enterprises where the intent of the governing body is that the cost of providing goods or services to the general public on a continuing basis be financed or recovered primarily through user charges; or (b) where the governing body has decided that the periodic determination of revenues earned, expenses incurred, and/or the change in net assets is appropriate for capital maintenance, public policy, management control, accountability, or other purposes.

D. Basis of Accounting

The financial statements have been prepared on the accrual basis of accounting. All sales are made for cash (or credit card) and recorded at the time of sale; revenues are recorded when earned, expenses are recognized when incurred. As permitted, the Board has elected to apply only applicable FASB Statements and Interpretations issued before November 30, 1989, in its proprietary operation, unless those pronouncements conflict with or contradict GASB pronouncements.

E. Use of Estimates

The preparation of financial statements in conformity with generally accepted accounting principles requires management to make certain estimates and assumptions that affect the reported amounts of assets and liabilities and disclosure of contingent assets and liabilities at the statement of net assets date, and reported amounts of revenues and expenses during the reporting period. Estimates are used to determine depreciation expense, the allowance for doubtful accounts and certain claims and judgment liabilities, among other accounts. Actual results may differ from those estimates.

F. Assets, Liabilities, and Net Position

(1) Deposits

All deposits of the ABC Board are made in board-designated official depositories and are collateralized as required by State law [G.S. 159-31]. The ABC Board may designate, as an official depository, any bank or savings association whose principal office is located in North Carolina. Also, the ABC Board may establish Time deposit accounts such as NOW and SuperNOW accounts, money market accounts, and certificates of deposit.

All the ABC Board's deposits are either insured or collateralized by using one of two methods. Under the Dedicated Method, all deposits that exceed the federal depository insurance coverage level are collateralized with securities held by the ABC Board's agent in the ABC Board's name. Under the Pooling Method, which is a collateral pool, all uninsured deposits are collateralized with securities held by the State Treasurer's agent in the name of the State Treasurer. Since the State Treasurer is acting in a fiduciary capacity for the ABC Board, these deposits are considered to be held by the ABC Board agent in the ABC Board's name. The amount of the pledged collateral is based on an approved averaging method for non-interest bearing deposits and the actual current balance for interest bearing deposits. Depositories using the Pooling Method report to the State Treasurer the adequacy of their pooled collateral covering uninsured deposits. The State Treasurer does not confirm this information with the ABC Board or the escrow agent. Because of the inability to measure the exact amount of collateral pledged for the ABC Board under the Pooling Method, the potential exists for under collateralization, and this risk may increase in periods of high cash flows. However, the State Treasurer of North Carolina enforces strict standards of financial stability for each depository that collateralizes public deposits under the Pooling Method.

At June 30, 2013, the ABC Board's deposits had a carrying amount of \$249,404 and a bank balance of \$281,662. The bank balance was covered by federal depository insurance on the first \$250,000 with the remainder covered by the pooling method.

(2) Investments

State law [G.S. 159-30(c)] authorizes the ABC Board to invest in obligations of the United States or obligations fully guaranteed both as to principal and interest by the United States; obligations of the State of North Carolina; bonds and notes of any North Carolina local government or public authority; obligations of certain non-guaranteed federal agencies; certain high quality issues of commercial paper and banker's acceptances; and the North Carolina Capital Management Trust (NCCMT), an SEC registered (2a-7) money market mutual fund.

(3) Cash and Cash Equivalents

For purposes of the statement of cash flows, the ABC Board considers all highly liquid investments (including restricted assets) with a maturity of three months or less when purchased to be cash equivalents.

(4) Accounts Receivable

All retail transactions are point of sale and the ABC board does not sell any items on credit. Claims receivable is the amount of breakage incurred by the store but still unreimbursed by the distiller.

(5) Inventories

Inventories are valued at the lower of cost (FIFO) or market.

(6) Property Plant and Equipment

Property and equipment are stated at cost and are being depreciated over their useful lives on a straight-line basis as follows:

<u>Asset</u>	<u>Useful Life</u>	<u>Cost</u>	<u>Accumulated Depreciation June 30, 2013</u>	<u>Depreciation Expense for Year</u>
Leasehold Improvement	10 Years	\$ 620	\$ 284	\$ 62
Equipment	5-10 Years	<u>188,908</u>	<u>91,928</u>	<u>22,757</u>
Total		<u>\$ 189,528</u>	<u>\$ 92,212</u>	<u>\$ 22,819</u>

When an asset is disposed of, the cost of the asset and the related accumulated depreciation are removed from the books. Any gain or loss on disposition is reflected in the earnings for the period.

(7) Net Position

Net position consists of the following:

- a. Restricted for law enforcement – this applies only when the Board employs its own ABC officer.
- b. Restricted for capital improvements – State Law [G.S. 18B-805 (d)] requires approval of the appointing authority to establish this account.
- c. Restricted for working capital – North Carolina Alcoholic Beverage Control Commission Rule [.0902] defines working capital as the total of cash, investments, and inventory less all unsecured liabilities. An ABC Board shall set its working capital requirements at not less than two weeks' average gross sales of the last fiscal year nor greater than three months' average gross sales of the last fiscal year. Average gross sales means gross receipts from the sale of alcoholic beverages less distributions required by State Law [G.S. 18B-805 (b) (2), (3) and (4)].
- d. Net Invested in capital assets – this component of net position consists of capital assets, including any restricted capital assets, net of accumulated depreciation and reduced by the outstanding balances of any borrowings that are attributable to the acquisition, construction, or improvement of those assets. If there are significant unspent related debt proceeds at year-end, the portion of the debt attributable to the unspent proceeds is not included in the calculation of invested in net capital assets. Rather, that portion of the debt is included in the same assets component as the unspent proceeds.
- e. Unrestricted net position – this component of net position consists of net position which does not meet the definition of restricted or net investment in capital assets debt.

Note 2. Stewardship, Compliance and Accountability

Compliance with N.C. General Statues

- A. The Asheboro ABC Board was in compliance with the General Statues of North Carolina at June 30, 2013.

Note 3. Detail Notes on All Funds

Pension Plan Obligations

A. Local Government Employees' Retirement System

Plan Description. Local Government Employees' Retirement System. The ABC Board

contributes to the statewide Local Government Employees' Retirement System (LGERS), a cost sharing multiple-employer defined benefit pension plan administered by the State of North Carolina. LGERS provides retirement and disability benefits to plan members and beneficiaries. Article 3 of G.S. Chapter 128 assigns the authority to establish and amend benefit provisions to the North Carolina General Assembly. The Local Government Employees' Retirement System is included in the Comprehensive Annual Report (CAFR) for the state of North Carolina. The State's CAFR includes financial statements and required supplementary information for LGERS. That report may be obtained by writing the office of the State Controller, 1410 Mail Service Center, Raleigh, NC 27699-1410, or by calling (919) 981-5454.

Fund Policy. Plan members are required to contribute six percent of their annual covered salary. The ABC Board is required to contribute at an actuarially determined rate. The ABC Board's current rate for employees not engaged in law enforcement is 6.74% of annual covered payroll. The contribution requirements of members and of the ABC Board are established and may be amended by the North Carolina General Assembly. The ABC Board's contribution to the LGERS for the years ended June 30, 2013, 2012, and 2011 was \$9,481, \$10,201, and \$9,553. The contributions made by the ABC Board equaled the required contributions for each year.

Note 4. Commitments

The ABC Board has elected to pay direct costs of employment securities benefits in lieu of employment security taxes on payroll. A liability for benefit payments could accrue in the year following discharge of employees.

Note 5. Leases

The ABC Board has an operating lease for its single facility at 700 South Fayetteville Street Asheboro, NC. The lease is a 10 year lease expiring December 15, 2018.

Lease Payment Schedule:

FYE June 30, 2014	\$71,952
FYE June 30, 2015	\$71,952
FYE June 30, 2016	\$71,952
FYE June 30, 2017	\$71,952
Thereafter	\$67,952

The Lessor is responsible for building insurance and property taxes. The table above is a summary of the lease obligations of the board according to the lease that was in place on June 30, 2013. During a review of the actions of the board subsequent to that date, it was discovered that the operating lease on the building was renegotiated. Beginning August 1, 2013, the monthly rental rate will be reduced by \$596 to \$5,400 per month.

Note 6. Related Party Transactions

There were no related party transactions for the fiscal year ended June 30, 2013.

Note 7. Vacation and Sick Leave Compensation

ABC Board policy is to give full time employees paid vacation days ranging from six days per year to twenty one days per year depending on length of employment. The amount of accrued vacation at year end was \$11,106.

Employees are not limited in the amount of sick leave that can be accumulated. Sick leave does not vest but unused sick leave accumulated at the time of retirement may be used in the determination of length of service for retirement benefit purposes. Since the Board has no obligation for accumulation of sick leave until it is actually taken, no accrual for sick leave has been made.

Note 8. Debt

The Board has no debt as of June 30, 2013. The board elected not to renew an existing line of credit with Community One Bank.

Note 9. Distributions of Income

As of June 30, 2013, the Board had not exceeded the maximum working capital requirement and was not required to make a profit distribution to the City of Asheboro. The board did however elect to make voluntary profit distributions to the City during the year totaling \$230,000.

State law [G.S. 18B-805] requires that the minimum distribution set aside in (c)(1) and any profit remaining after deducting amounts required for law enforcement and alcohol education and retaining proper working capital, be paid quarterly to the appointing authority.

Note 10. Law Enforcement and Alcohol Education Expenses

The Board is required by law to expend at least 5% of its profits for law enforcement and 7% of its profits for the treatment of alcoholism or substance abuse, or for research or education on alcohol or substance abuse. Profits are defined by law for these calculations as income before law enforcement and education expenses, less 3 ½% markup provided in G.S. 18B-804 (b) (5) and the bottle charge provided for in G.S. 18B-804 (b) (6b). The following schedule summarizes the Board's expenditures for these items during 2013:

Income before required distributions	\$ 314,579
Less 3 1/2% tax & bottle charge	83,440
Profit subject to expense percentages	<u>\$ 231,139</u>
Law Enforcement Expenditures	\$ 11,557
Actual % of Profit	5%
Provision for Alcoholic Education & Rehabilitation	\$ 16,179
Actual % of Profit	7%

Note 11. Disbursement of Taxes Included in Selling Price

A state excise tax, at the rate of 30% on the retail (net sales) price is charged monthly on liquor sales (excluding wine sales).

Transactions for this account for the years are summarized as follows:

	<u>2013</u>	<u>2012</u>
Taxes payable – beginning of year	\$ 51,588	\$ 45,754
Taxes collected during the year	625,569	598,447
Taxes remitted to the Department of Revenue	<u>626,819</u>	<u>592,613</u>
Taxes payable – end of year	<u>\$ 50,338</u>	<u>\$ 51,588</u>

The excise tax is computed in accordance with G.S. 18B-805 (i).

The accrued North Carolina excise tax at June 30, 2013, was remitted to the North Carolina Department of Revenue on July 15, 2013.

A bottle charge of one cent on each bottle containing 50 milliliters or less and five cents on each bottle containing more than 50 milliliters is collected and distributed monthly to the county commissioners for alcohol education and rehabilitation. For the year ended June 30, 2013, payments to the county were based on the following bottle sales:

For the year ended June 30, 2013			
Regular Bottles	172913	@.05	\$ 8,646
Mixed Beverage Bottles	16209	@.05	810
Miniature Bottles	59507	@.01	<u>595</u>
Total Payment for the year ended June 30, 2013			<u>\$ 10,051</u>

A "mixed beverage tax" at the rate of \$20.00 per 4 liters is charged on the sale of liquor to be resold as mixed beverages. One-half of the mixed beverage tax is submitted monthly to the North Carolina Department of Revenue. Five percent of the mixed beverage tax is submitted monthly to the NC Department of Human Resources.

The mixed beverage taxes for the years ended June 30, 2013 and 2012 was:

	<u>2013</u>	<u>2012</u>
NC Department of Revenue (50%)	\$ 31,159	\$ 32,935
NC Department of Human Resources (5%)	3,116	3,294
Profit Retained (45%)	<u>28,043</u>	<u>29,642</u>
Total	<u>\$ 62,318</u>	<u>\$ 65,871</u>

At June 30, taxes payable were:

	<u>2013</u>	<u>2012</u>
NC Department of Revenue - Excise Tax	\$ 50,338	\$ 51,588
NC Department of Revenue - Mixed Beverage	2,493	2,907
NC Department of Human Resources - Mixed Beverage Tax	249	291
Randolph County - Bottle Tax	<u>812</u>	<u>839</u>
Total	<u>\$ 53,892</u>	<u>\$ 55,625</u>

Note 12. Surcharge Collected

The total amount of surcharge collected for the fiscal year was \$15,128. (The rate is 80 cents per case sold.)

Note 13. Liquor Sales Tax

The total amount of sales tax collected by the ABC Board and remitted to the Department of Revenue for the fiscal year was \$178,589. The current sales tax rate is 7%.

Note 14. Retail Outlets

The Asheboro Board of Alcoholic Beverage Control operated with one retail outlet:

Asheboro, North Carolina	
Gross Sales	\$2,896,748
Changes in Net Assets	\$56,854

Note 15. Working Capital

The ABC Board is required by the Alcoholic Beverage Control Commission rule [.0902] to set its working capital requirements at not less than two weeks average gross sales of the last fiscal year. (Gross sales are gross receipts from the sale of alcoholic beverages less distributions as defined in G.S. 18B-805(b)(2), (3), and (4)).

The board's position on this requirement is as follows:

Minimum Amount	\$ 85,202
Maximum Amount	\$ 553,810
Actual Amount	\$ 401,682

The board has met the minimum amount of working capital.

Note 16. Breakage Expense

Breakage expense absorbed by the Board for the year was \$-0-.

Note 17. Subsequent Events

Subsequent events have been evaluated through September 13, 2013, which is the date the financial statements were available to be issued.

Note 18. Risk Management

The Board is exposed to various risks of loss related to torts; theft of, damage to, and destruction of assets; errors and omissions; injuries to employees; and natural disasters. The Board has property, general liability, liquor legal liability insurance and worker's compensation coverage and employee health coverage.

There have been no significant reductions in insurance coverage from coverage in the prior year and settled claims have not exceeded coverage in any of the past three fiscal years.

In accordance with G.S. 18B-700(i), each board member is bonded in the amount of \$50,000 secured by a corporate surety.

In accordance with G.S. 18B-803(b) and (c), the store manager(s) is bonded for \$50,000. All other employees who have access to funds are bonded under a blanket bond for \$50,000.

SUPPLEMENTARY INFORMATION

Asheboro Board of Alcoholic Beverage Control
Schedule of Store Expenses
For the Years Ended June 30, 2013
and June 30, 2012

	<u>2013</u>	<u>2012</u>
Salaries & Wages	\$ 201,978	\$ 197,221
Payroll Tax	15,758	15,585
Group Insurance	8,930	8,390
Retirement	9,481	10,201
Cash Short / Over	(349)	
Utilities	4,478	4,530
Repairs & Maint - Bldg	510	1,355
Repairs & Maint - Equip	1,740	155
Janitorial Service	150	526
Store Supplies	3,208	5,379
Insurance - General	10,368	9,865
Credit Card Fees	27,707	14,048
Rent	35,976	35,976
Drug Testing Expense	385	410
Training	900	483
Total Store Expenses	<u>\$ 321,220</u>	<u>\$ 304,124</u>

Asheboro Board of Alcoholic Beverage Control
Schedule of Administrative Expenses
For the Years Ended June 30, 2013
and June 30, 2012

	<u>2013</u>	<u>2012</u>
Telephone	\$ 3,789	\$ 3,891
Board Members Fees	3,600	3,600
Office Supplies	2,123	2,237
Dues & Subscriptions	295	676
Travel	728	325
Internet Services	1,570	1,069
Postage	455	488
Maintenance Agreements	4,132	3,489
Bank Service Charges	500	1,005
Misc Expense	134	45
Professional Fees	4,050	4,288
Alarm Service	865	987
Total Administrative Expenses	<u>\$ 22,241</u>	<u>\$ 22,100</u>

Asheboro Board of Alcoholic Beverage Control
Schedule of Warehouse Expenses
For the Years Ended June 30, 2013
and June 30, 2012

	<u>2013</u>	<u>2012</u>
Utilities	\$ 4,477	\$ 4,530
Rent	35,976	35,976
Total Warehouse Expenses	<u>\$ 40,453</u>	<u>\$ 40,506</u>

Asheboro Board of Alcoholic Beverage Control
Schedule of Revenue and Expenditures – budget and Actual
Year Ended June 30, 2013

	2013 Original Budget	2013 Revised Budget	2013 Actual	Variance Positive (Negative)
Revenues				
Operating Revenues:				
Sales	\$ 2,910,000	\$ 2,910,000	\$ 2,896,746	\$ (13,254)
Non-operating Revenues:				
Interest	<u>300</u>	<u>300</u>	<u>270</u>	<u>(30)</u>
Total Revenues	<u>\$ 2,910,300</u>	<u>\$ 2,910,300</u>	<u>\$ 2,897,016</u>	<u>\$ (13,284)</u>
Expenditures:				
Taxes based on revenue:	\$ 814,800	\$ 814,800	\$ 670,736	\$ 144,064
Cost of Goods Sold	1,424,940	1,424,940	1,504,972	(80,032)
Operating Expenses:				
Salaries and Benefits	232,000	232,000	223,989	8,011
Payroll Taxes	17,000	17,000	15,758	1,242
Rent	71,952	71,952	71,952	-
Utilities	20,000	20,000	13,940	6,060
Repairs & Maintenance	7,000	7,000	6,385	615
Office Store Supplies	13,300	13,300	5,936	7,364
Insurance - general & bonds	14,000	14,000	10,368	3,632
Travel	2,250	2,250	1,628	622
Professional fees	8,446	8,446	5,805	2,641
Dues and subscriptions	1,545	1,545	292	1,253
Bank fees and charges	37,699	37,699	28,207	9,492
Interest expense				-
Miscellaneous expense	2,500	2,500	(349)	2,849
Contingencies	2,500	2,500		2,500
Capital Outlay	<u>25,000</u>	<u>25,000</u>	<u>14,390</u>	<u>10,610</u>
Total Expenditures	<u>\$ 2,694,932</u>	<u>\$ 2,694,932</u>	<u>\$ 2,574,009</u>	<u>\$ 120,923</u>
Distributions				
Law enforcement	\$ 13,408	\$ 13,408	\$ 11,557	\$ 1,851
Alcohol education & rehab	15,960	15,960	16,179	(219)
Municipal	<u>186,000</u>	<u>186,000</u>	<u>230,000</u>	<u>(44,000)</u>
Total	<u>\$ 215,368</u>	<u>\$ 215,368</u>	<u>\$ 257,736</u>	<u>\$ (42,368)</u>
Total Expenditures & Distributions	<u>\$ 2,910,300</u>	<u>\$ 2,910,300</u>	<u>\$ 2,831,745</u>	<u>\$ 78,555</u>
Revenues over Expenditures and other Financing (Uses)	<u>\$ -</u>	<u>\$ -</u>	<u>\$ 65,271</u>	<u>\$ 65,271</u>
Reconciliation from budgetary basis (modified accrual) to full accrual:				
Reconciling items:				
Depreciation			22,819	
Capital Outlay			<u>14,390</u>	
Change in net position			<u>\$ 56,842</u>	

Minutes of the meeting of the Asheboro Alcoholic Beverage Control Board held on November 4, 2013

Item 5 (c)

The Asheboro ABC Board met on November 4, 2013, at 5:30 PM, in the Board office, 700 South Fayetteville Street, Asheboro, NC.

Present were Board Members Steve Knight and Bob Morrison and General Manager Rodney Johnson (GM). Chair Brooke Schmidly attended via telephone conference. A quorum being present, the Chair called the meeting to order for the transaction of business and business transacted as follows:

The Chair inquired as to any known conflict of interest, appearance of a conflict of interest, or objections concerning agenda items before the Board; after the Chair and both Board members voiced having no conflict, and there being no objection, the agenda was adopted.

The Board reviewed and there being no objection, approved and signed the Minutes from the October 7, 2013, regular meeting.

Steve Knight and the GM reviewed Board finances and reported all finances remain consistent.

Asheboro ABC Policy Section 33 provides the Board may grant bonus pay at such times as it deems appropriate and will consider COLA adjustments annually. After the Board discussed sales and profits, the Chair moved payment of a Christmas bonus equal to 3% of an employee's salary during the period beginning December 1, 2012, and ending November 30, 2013, with a minimum payment of \$100 and that no COLA adjustment be granted. The Board approved the motion.

The GM reviewed correspondence from ADS thanking the Board for taking the time to consider its grant application.

The ABC Commission now has an internet link available for on-line webinar ethics training. Board's can purchase the on-demand webinar to be used as needed.

The Board heard reports from the General Manager concerning the following issues:

1. The Change-Fund Report ending October 31, 2013, was reviewed with nothing remarkable noted.
2. Asheboro ABC sales statistics comparing:
 - September 2013 sales with the previous month indicate:
 - An overall +11.3% change (all sales and tax collections)
 - September 2013 sales with sales from the same month last year indicate:
 - Retail Sales +3.5%
 - Mixed Beverage Sales: +13.2%

- Sales Tax Collections: +3.6%
- Overall Collections: +4.6%

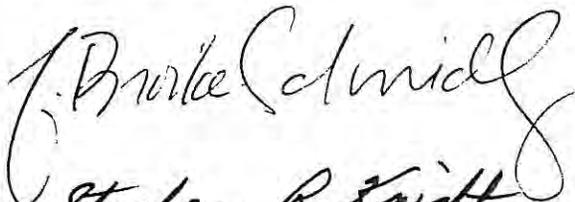
- September 2013 bottle sales with bottle sales from the same month last year indicate:
 - Retail Bottle Sales: +7.5%
 - Mixed Beverage Bottle Sales: +13.6%
 - Overall Bottle Sales: +7.9%

Charts reflecting sales histories were handed out to Board members for review and discussion.

The next regular Asheboro ABC Board meeting will be held Monday, December 6, 2013.

There being no further business, the meeting was adjourned.

Prepared by Rodney Johnson, GM, and Approved by the Board 12-2-13 
GM




MUNICIPAL GENERAL ELECTION
November 5, 2013
ABSTRACT OF VOTES
FOR
STATE OF NORTH CAROLINA

Item 5 (d)

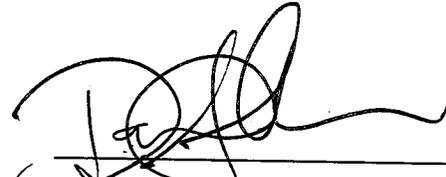
INSTRUCTIONS

THREE COPIES are to be made. One copy is to be signed by the County Board of Elections and delivered by them, when the canvass is concluded, to the City Clerk (GS 163-300). One copy is to be retained by the County Board of Elections. One copy is to be mailed on canvass day to the Chairman of the State Board of Elections, Raleigh, North Carolina (GS 163-182.6).

STATE OF NORTH CAROLINA
COUNTY OF RANDOLPH

The County Board of Elections for said county, having opened, canvassed, and judicially determined the original returns of the election in the precincts in this county, held as above stated, do hereby certify that the above is a true abstract thereof, and contains the number of legal ballots cast in each precinct for each office or referendum named, the name of each person or choice voted for, their party affiliation (where applicable), and the number of votes cast for each person or choice for the item named.

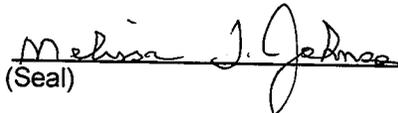
This is the 12th day of November 2013



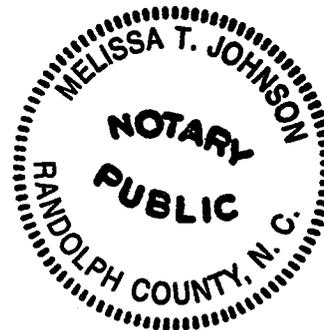
John W. McArthur

This day personally appeared before me, Darren C. Allen, Chairman of the County Board of Elections, who being duly sworn, says the abstract of votes herein contained is true and correct, according to the returns made to said Board.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed My notarial seal this the 12th day of November, 2013.



(Seal)



My Commission (if any) Expires: 6-11-2014

Official Results Randolph Co.
2013 Municipal Election

STATISTICS	ASHEBORO MAYOR					ASHEBORO COUNCIL					FRANKLINVILLE MAYOR		FRANKLINVILLE COMMISSIONER					
	VOTERS REGISTERED	CASUALS	Jackie O'Connell	David H. Smith	WRITTE-IN	Clark Bell	Hames Lindar	Mike Hunter	B. Mofkett	WRITTE-IN	Perry L. Conner	WRITTE-IN	Paul Dunn	D. Goodwin	Sharon A. Grose	A. C. Hurley	(Mac) L. Whately	WRITTE-IN
AE 05 ASHEBORO EAST	2257	117	7	85	0	81	68	62	66	4								
AE 06 ASHEBORO EAST	2115	268	37	218	8	223	192	190	180	7								
AN 09 ASHEBORO NORTH	1546	81	22	53	5	59	67	64	53	6								
AN 10 ASHEBORO NORTH	2065	94	23	65	6	78	77	70	67	8								
AS 04 ASHEBORO SOUTH	2049	170	25	124	4	111	102	111	103	8								
AS 11 ASHEBORO SOUTH	1729	79	9	48	2	47	47	38	36	1								
AW 07 ASHEBORO WEST	2158	122	17	92	2	96	89	90	76	2								
AW 08 ASHEBORO WEST	1709	111	24	77	1	72	77	71	71	5								
AW 12 ASHEBORO WEST	1597	143	5	99	2	98	63	73	73	5								
BC BACK CREEK	234	3																
DR 20 DEEP RIVER	524	55									38	9	21	41	35	40	17	5
GR GRANT	292	13	2	7	3	6	13	4	5	0								
LB LIBERTY	1608	236																
LC LEVEL CROSS	29	4																
RM RAMSEUR	944	141																
RN 32 RANDLEMAN	2148	423	1	6	0	7	7	6	7	0								
RN 33 RANDLEMAN	965	190																
SO 34 SOUTHERN	201	81																
ST STALEY	242	22																
SW 18 SOUTHWEST	2	0	0	0	0	0	0	0	0	0								
TR 28 TRINITY	1454	274																
TR 37 TRINITY	915	180																
TR 39 TRINITY	2404	556																
JG 15 UNION GROVE	329	9	2	3	0	4	5	5	4	0								
JG 16 UNION GROVE	260	13	0	8	0	8	7	7	8	2								
TOTALS	29776	3385	174	885	33	890	814	791	749	48	38	9	21	41	35	40	17	5

Official Results Randolph Co.
2013 Municipal Election

LIBERTY MAYOR		LIBERTY COMMISSIONER						LIBERTY COMMISSIONER (UNEX)		RAMSEUR COMMISSIONER			RAMSEUR COMMISSIONER (UNEX)					
L C T r r y	W R I T E - I N	C h r i s t o p h e r	R o b i n M F r a d y	T y s o n E N i x o n	J Q u i n c y S i l e r	F i l m o o r e Y o r k	W R I T E - I N	M a r k (M J) J o n e s	W R I T E - I N	R o b e r t	H e s s e l m e y è r	L e e H o o v e r J r y	J C P a r r i s h	W R I T E - I N	E r n i e M i t c h e l l	M o r r i s R u d i c k	W R I T E - I N	
LB LIBERTY	191	19	79	96	147	81	204	7	170	4								
RM RAMSEUR											79	96	70	6	81	51	1	
TOTALS	191	19	79	96	147	81	204	7	170	4	79	96	70	6	81	51	1	1

Official Results Randolph Co.
2013 Municipal Election

RANDLEMAN MAYOR			RANDLEMAN ALDERMAN WARD 2		RANDLEMAN ALDERMAN WARD 3			SEAGROVE MAYOR		SEAGROVE COMMISSIONER								
Bucky	John	Wright	Allred	Wright	Keith	Eric	Wright	Roy	Wright	Pam	Jason	David	Ferr	D. G	Barbara	Chris	Grab	Wright
Jernigan	Anthony	Thelton	Melissa	Blair	Raymond	Ward	Edmonds	Edmonds	Edmonds	Allen	Auman	Robert	Barbara	Graves	Luther	Mullin	Jodi	Sapper
LC LEVEL CROSS	0	4	0	2	0	1	3	0										
RN 32 RANDLEMAN	201	214	0	278	86	131	277	2										
RN 33 RANDLEMAN	104	86	0	93	60	65	122	0										
SO 34 SOUTHERN									60	10	49	63	34	17	64	57	55	14
TOTALS	305	304	0	373	146	197	402	2	60	10	49	63	34	17	64	57	55	14

TRINITY COUNCIL WARD 3			TRINITY COUNCIL WARD 4			ASHEBORO BOARD OF EDUCATION				ASHEBORO BOARD OF EDUCATION (UNEX)			
T Y l e r E a r n s t	J e s s e R . H i l l	W R I T E - I N	T o m m y J o h n s o n	D o n P a y n e	W R I T E - I N	G u s A g u d e l o	R . P h i l l e e k	C r a n f o r d L i n f o r d	S t e v e J o n e s	W R I T E - I N	W . K e l l y H a r r i s	W R I T E - I N	
AE 05 ASHEBORO EAST						56	87	78	68	1	84	0	
AE 06 ASHEBORO EAST						171	194	201	189	0	197	3	
AN 09 ASHEBORO NORTH						31	42	43	44	4	39	1	
AN 10 ASHEBORO NORTH						46	67	65	58	6	62	4	
AS 04 ASHEBORO SOUTH						104	117	125	117	3	138	1	
AS 11 ASHEBORO SOUTH						44	54	59	58	6	68	2	
AW 07 ASHEBORO WEST						84	98	104	91	1	102	2	
AW 08 ASHEBORO WEST						65	85	89	73	2	80	0	
AW 12 ASHEBORO WEST						80	92	112	104	0	115	1	
BC BACK CREEK						1	1	3	1	0	1	0	
GR GRANT						0	0	0	0	0	0	0	
TR 28 TRINITY	91	174	0	100	170	0							
TR 37 TRINITY	55	122	0	65	112	0							
TR 39 TRINITY	238	312	0	254	286	1							
UG 15 UNION GROVE						8	7	8	8	0	7	0	
UG 16 UNION GROVE						3	5	4	4	1	3	2	
TOTALS	384	608	0	419	568	1	693	849	891	815	24	896	16

MUNICIPAL ELECTION
November 5th, 2013
ABSTRACT OF WRITE-IN VOTES
STATE OF NORTH CAROLINA

INSTRUCTIONS

THREE COPIES are to be made. One copy is to be signed by the County Board of Elections and delivered by them, when the canvass is concluded, to the City Clerk (GS 163-300). One copy is to be retained by the County Board of Elections. One copy is to be mailed on canvass day to the Chairman of the State Board of Elections, Raleigh, North Carolina (GS 163-182.6).

STATE OF NORTH CAROLINA
COUNTY OF RANDOLPH

Write-in votes must be accounted for if an individual receives 5 or more votes. All other write-in votes shall be accumulated and placed under Miscellaneous

OFFICE: Asheboro Mayor	TOTAL WRITE-IN VOTES: 33
NAME: Joe Deaton	WRITE-IN VOTES: 25
MISCELLANEOUS VOTES: 8	

OFFICE: Asheboro City Council	TOTAL WRITE-IN VOTES: 48
MISCELLANEOUS VOTES: 48	

OFFICE: Asheboro School Board	TOTAL WRITE-IN VOTES: 24
MISCELLANEOUS VOTES: 24	

OFFICE: Asheboro School Board (unexpired term)	TOTAL WRITE-IN VOTES: 16
MISCELLANEOUS VOTES: 16	

OFFICE: Franklinville Mayor	TOTAL WRITE-IN VOTES: 9
MISCELLANEOUS VOTES: 9	

OFFICE: Franklinville Town Commissioner	TOTAL WRITE-IN VOTES: 5
MISCELLANEOUS VOTES: 5	

OFFICE: Liberty Mayor
MISCELLANEOUS VOTES: 19

TOTAL WRITE-IN VOTES: 19

OFFICE: Liberty Town Commissioner
MISCELLANEOUS VOTES: 7

TOTAL WRITE-IN VOTES: 7

OFFICE: Liberty Town Commissioner (unexpired term)
MISCELLANEOUS VOTES: 4

TOTAL WRITE-IN VOTES: 4

OFFICE: Ramseur Town Commissioner
MISCELLANEOUS VOTES: 6

TOTAL WRITE-IN VOTES: 6

OFFICE: Ramseur Town Commissioner (unexpired term)
MISCELLANEOUS VOTES: 1

TOTAL WRITE-IN VOTES: 1

OFFICE: Randleman Alderman Ward 2
NAME: Craig Hancock
NAME: Keith Fogleman
MISCELLANEOUS VOTES: 4

TOTAL WRITE-IN VOTES: 146
WRITE-IN VOTES: 136
WRITE-IN VOTES: 6

OFFICE: Randleman Alderman Ward 3
MISCELLANEOUS VOTES: 2

TOTAL WRITE-IN VOTES: 2

OFFICE: Seagrove Mayor
NAME: Mike Walker
MISCELLANEOUS VOTES: 3

TOTAL WRITE-IN VOTES: 10
WRITE-IN VOTES: 7

OFFICE: Seagrove Town Commissioner
MISCELLANEOUS VOTES: 14

TOTAL WRITE-IN VOTES: 14

OFFICE: Staley Town Commissioner
MISCELLANEOUS VOTES: 3

TOTAL WRITE-IN VOTES: 3

OFFICE: Trinity Mayor
NAME: Karen Bridges
MISCELLANEOUS VOTES: 2

TOTAL WRITE-IN VOTES: 195
WRITE-IN VOTES: 19

OFFICE: Trinity Council Ward 4
MISCELLANEOUS VOTES: 1

TOTAL WRITE-IN VOTES: 1

Committee Appointments Made During Organizational Meeting of December 8, 2011

Committee Appointments Proposed to Remain Unchanged

- I. Finance and Public Safety Committee
 1. Mayor, Chair
 2. Linda Carter
 3. Clark Bell
 4. Charles Swiers

- II. Public Works Committee
 1. Mayor Pro Tem, Chair
 2. Eddie Burks
 3. Mike Hunter
 4. Walker Moffitt

- III. Personnel Evaluation Committee
 1. Mayor, Chair
 2. Mayor Pro Tem
 3. Linda Carter

- IV. Tourism and Marketing Committee
 1. Mayor, Chair
 2. Eddie Burks
 3. Walker Moffitt

- V. Identity Theft Prevention Program Committee
 1. Mayor, Chair
 2. Linda Carter
 3. Mike Hunter

NC League of Municipalities Voting Delegate: Mayor Pro Tem

Piedmont Triad Regional Council Delegate: Mayor

City Council Meetings 2014

January 9

February 6

March 6

April 10

May 8

June 5

July 10

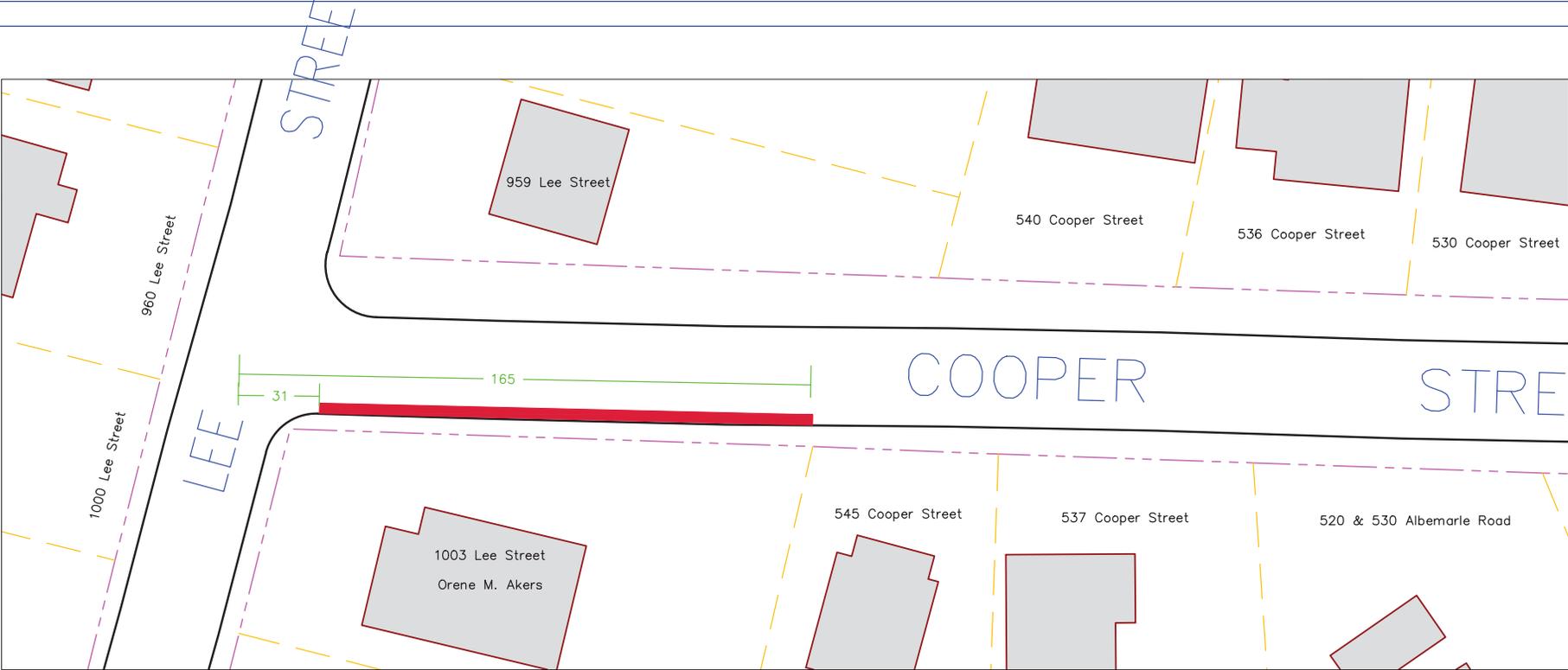
August 7

September 11

October 9

November 6

December 4



 No Parking

Parking Plan For Cooper Street
 South Side, Between Lee and South Park Streets
 City of Asheboro, North Carolina
 November, 2013 Scale 1" = 30'





Item 7 (c)

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

PAT MCCRORY
GOVERNOR

ANTHONY J. TATA
SECRETARY

November 15, 2013

STATE PROJECT: 17BP.8.R.19
COUNTY: Randolph
DESCRIPTION: Bridge No. 419 over Penwood Branch on SR 2159 (Draper Street)
SUBJECT: Utility Agreement – City of Asheboro

Mr. John Ogburn
Asheboro City Manager
PO Box 1106
Asheboro, NC 27204

Dear Mr. Ogburn:

Enclosed are duplicate originals of a Utility Agreement to adjust and relocate municipally owned water and sewer lines on the above project.

After you have reviewed this agreement, please have the appropriate authority approve and execute the agreement. Once signed, both originals should be returned to the Utility Agent for Departmental execution. One fully executed agreement will be sent back to you for your file.

In order to keep the project on schedule, please approve and forward the signed agreement back to this office no later than December 31, 2013. Should this office not receive the agreement by the requested date, the project is subject to be delayed.

If revisions are necessary, please email Ed Reams, Area Utility Agent, at ereams@ncdot.gov or call (919) 707-6995, to make the appropriate changes. I will promptly forward the revised agreement back to you for review and execution.

Sincerely,

J. Robert Memory
State Utility Agent

Enclosures

cc: Mr. Rob Stone II, PE, Division Engineer
Mr. Reuben Blakely, PE, Resident Engineer
Mr. Ed Reams, Area Utility Agent
Mr. Jamie Yow, Division Utility Agent
Ms. Sarah Garcia, Contract Officer

MAILING ADDRESS:
NC DEPARTMENT OF TRANSPORTATION
UTILITIES UNIT
1555 MAIL SERVICE CENTER
RALEIGH NC 27699-1555

TELEPHONE: 919-707-6690
Fax: 919-250-4151

WEBSITE: WWW.NCDOT.ORG/DOH

LOCATION:
CENTURY CENTER COMPLEX
BUILDING B
1020 BIRCH RIDGE DRIVE
RALEIGH NC 27610

NORTH CAROLINA
RANDOLPH COUNTY

UTILITY AGREEMENT

DATE: 11/07/2013

NORTH CAROLINA DEPARTMENT OF
TRANSPORTATION

AND

WBS Elements: 17BP.8.R.19

CITY OF ASHEBORO

THIS AGREEMENT is made and entered into on the last date executed below, by and between the North Carolina Department of Transportation, an agency of the State of North Carolina, hereinafter referred to as the "Department" and the City of Asheboro, hereinafter referred to as the "Municipality";

WITNESSETH:

WHEREAS, the Department has prepared and adopted plans to make certain street and highway improvements under Project 17BP.8.R.19, in Randolph County, said plans consists of Bridge No. 419 over Penwood Branch on SR 2159 (Draper Street); Randolph County; said project having a right-of-way width as shown on the project plans on file with the Department's office in Raleigh, North Carolina; and,

WHEREAS, the parties hereto wish to enter into an agreement for certain utility work to be performed by the Department's construction contractor with full reimbursement by the Municipality for the costs thereof as hereinafter set out.

NOW, THEREFORE, it is agreed as follows:

The Department shall place provisions in the construction contract for Project 17BP.8.R.19 in Randolph County, for the contractor to adjust and relocate water and sewer lines. Said work shall be accomplished in accordance with project special provisions attached hereto as Exhibit "A", cost estimate attached hereto as Exhibit "B" and the plan sheets attached hereto as Exhibit "C".

1. The Municipality shall be responsible for water and sewer lines cost as shown in red on the attached Exhibit "C". The estimated cost to the Municipality is \$130,184.00 as shown on the attached Exhibit "B". It is understood by both parties that this is an estimated cost and is subject to change. The Municipality shall reimburse the Department for said costs as follows:

- A. Upon completion of the highway work, the Department shall submit an itemized invoice to the Municipality for cost incurred. Billing will be based upon the actual bid prices and actual quantities used.
 - B. Reimbursement shall be made by the Municipality in one final payment within sixty (60) days of said invoice.
 - C. If the Municipality does not pay said invoice within sixty (60) days of the date of the invoice, the Department shall charge interest on any unpaid balance at a variable rate of the prime plus (1%) in accordance with G.S. 136-27.3.
 - D. Said interest rate shall be set upon final execution of the Agreement by the Department. The Municipality will be notified of the set interest rate by the Department's approval letter upon receipt of the fully executed agreement.
 - E. Any cost incurred due to additional utility work requested by the Municipality after award of the construction contract, shall be solely the responsibility of the Municipality. The Municipality shall reimburse the Department 100% of the additional utility cost.
2. In the event the Municipality fails for any reason to pay the Department in accordance with the provisions for payment hereinabove provided, North Carolina General Statute 136-41.3 authorizes the Department to withhold so much of the Municipality's share of funds allocated to said Municipality by North Carolina General Statute, Section 136-41.1, until such time as the Department has received payment in full.
3. Upon the satisfactory completion of the relocations and adjustments of the utility lines covered under this Agreement, the Municipality shall assume normal maintenance operations to the said utility lines. Upon completion of the construction of the highway project, the Municipality shall release the Department from any and all claims for damages in connection with adjustments made to its utility lines; and, further, the Municipality shall release the Department of any future responsibility for the cost of maintenance to said utility lines. Said releases shall be deemed to be given by the Municipality upon completion of construction of the project and its acceptance by

the Department from its contractor unless the Municipality notifies the Department, in writing, to the contrary prior to the Department's acceptance of the project.

4. It is further agreed that the following provisions shall apply regarding the utilities covered in this Agreement.
 - A. The Municipality obligates itself to service and to maintain its facilities to be retained and installed over and along the highway within the Department's right-of-way limits in accordance with the mandate of the North Carolina General Statutes and such other laws, rules, and regulations as have been or may be validly enacted or adopted, now or hereafter.
 - B. If at any time the Department shall require the removal of or changes in the location of the encroaching facilities which are being relocated at the Municipality's expense, the Municipality binds itself, its successors and assigns, to promptly remove or alter said facilities, in order to conform to the said requirement (if applicable per G.S. 136-27.1), without any cost to the Department.
5. By Executive Order 24, issued by Governor Perdue, and N.C. G.S. § 133-32, it is unlawful for any vendor or contractor (i.e. architect, bidder, contractor, construction manager, design professional, engineer, landlord, offeror, seller, subcontractor, supplier, or vendor), to make gifts or to give favors to any State employee of the Governor's Cabinet Agencies (i.e., Administration, Commerce, Correction, Crime Control and Public Safety, Cultural Resources, Environment and Natural Resources, Health and Human Services, Juvenile Justice and Delinquency Prevention, Revenue, Transportation, and the Office of the Governor).

IN WITNESS WHEREOF, this Agreement has been executed, in duplicate, the day and year heretofore set out, on the part of the Department and the Municipality by authority duly given.

L.S. ATTEST: CITY OF ASHEBORO
BY: _____ BY: _____
TITLE: _____ TITLE: _____
DATE: _____

N.C.G.S. § 133-32 and Executive Order 24 prohibit the offer to, or acceptance by, any State Employee of any gift from anyone with a contract with the State, or from any person seeking to do business with the State. By execution of any response in this procurement, you attest, for your entire organization and its employees or agents, that you are not aware that any such gift has been offered, accepted, or promised by any employees of your organization.

Approved by _____ of the local governing body of the City of Asheboro as attested to by the signature of Clerk of said governing body on _____ (Date)

This instrument has been pre-audited in the manner required by the Local Government Budget and Fiscal Control Act.

(SEAL)

(FINANCE OFFICER)

Federal Tax Identification Number

Remittance Address:

City of Asheboro

DEPARTMENT OF TRANSPORTATION

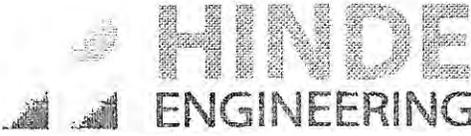
BY: _____
(CHIEF ENGINEER)

DATE: _____

APPROVED BY BOARD OF TRANSPORTATION ITEM O: _____ (Date)

PROJECT SPECIAL PROVISIONS
Utility Construction

EXHIBIT A

 <p>License No. C-2639 7520 E. Independence Blvd. Suite 230 Charlotte, NC 28227 (704) 814-4407</p>	
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Revise the 2012 Standard Specifications as follows:

Utility Owner's Contact Information:

Page 15-1, Sub-article 1500-2, Cooperation with the Utility Owner, paragraph 2:
add the following sentences:

The utility owner is City of Asheboro. The contact person is Michael Leonard and he can be reached by phone at 336-626-1201 ext. 232 or mleonard@ci.asheboro.nc.us. Contractor shall coordinate all water and sewer construction with City of Asheboro Water and Sewer Maintenance, Andrew Connor 336-626-1234 ext. 2201.

Page 15-6, Sub-article 1510-3 (B), Testing and Sterilization:
change the allowable leakage formula to:

$$W = LD\sqrt{P} \div 148,000$$

Page 15-6, Sub-article 1510-3 (B) Line 32, Testing and Sterilization, seventh paragraph:

delete the words "may be performed concurrently or"
and replace with "shall be performed".

SPU-1; BREAK DOWN AND REBUILD MANHOLE:

Manholes that will require rebuilding shall have the frame and cover removed, the manhole taper removed, and necessary manhole wall removed. Reconstruction shall include rebuilding manhole wall, manhole taper, and replacing frame and cover. All work shall be approved by the Engineer.

Manhole construction shall also conform to the applicable requirements of Section 840 of the Standard Specifications.

Measurement and Payment:

Payment for removing and replacing manhole frame and cover, removing and rebuilding manhole taper and removing and rebuilding manhole wall will be based on a unit price for each manhole.

The quantity of sewer manholes rebuilt and accepted will be measured and paid for at the contract unit price each for "Break Down and Rebuild Manhole". Such price and payments will be full compensation for all labor, materials, breaking down manhole, new manhole construction, steps, excavation, backfilling, and incidentals necessary to complete the work as required.

Payment will be made under:

Pay Item	Pay Unit
Break Down and Rebuild Manhole	EA

EXHIBIT B

Hinde Proj. Number <u>2011113.00</u>	
NCDOT Proj. Number <u>17BP.8.R.19</u>	
Project <u>Bridge No. 419 over Penwood Branch on SR 2159</u>	
Location <u>Randolph County</u> (City of Asheboro Portion)	
7520 E. Independence Blvd., Suite 230 Charlotte, North Carolina 28227 (704) 814-4407 (office) (704) 814-9042 (fax)	Calculated By <u>JNA</u> Date <u>6/27/2013</u> Reviewed By <u>KSH</u> Date <u>6/27/2013</u>

TYPE	ITEM NUMBER	SECTION	ITEM DESCRIPTION	UNITS	QUANTITY	UNIT PRICE	AMOUNT
D	0318000000-E	300	Foundation Conditioning Material, Minor Strs	TON	99		
U	5325800000-E	1510	8" Water Line	LF	335		
U	5672000000-N	1515	Relocate Fire Hydrant	EA	1		
U	5678600000-E	1515	8" Line Stop	EA	2		
U	5691300000-E	1520	8" Sanitary Gravity Sewer	LF	130		
U	5691500000-E	1520	12" Sanitary Gravity Sewer	LF			
U	5775000000-E	1525	4' Dia Utility Manhole	EA			
U	5776000000-E	1525	5' Dia Utility Manhole	EA			
U	5781000000-E	1525	Utility Manhole Wall, 4' Dia	LF			
U	5782000000-E	1525	Utility Manhole Wall, 5' Dia	LF			
U	5801000000-E	1530	Abandon 8" Utility Pipe	LF	482		
U	5802000000-E	1530	Abandon 10" Utility Pipe	LF			
U	5804000000-E	1530	Abandon 12" Utility Pipe	LF			
U	5828000000-N	1530	Remove Utility Manhole	EA			
U	5835700000-E	1540	16" Encasement Pipe, 0.250" Thick	LF	70		
U	5836000000-E	1540	24" Encasement Pipe, 0.250" Thick	LF			
U	5871900000-E	1550	Trenchless Installation of 16" In Soil	LF	35		
U	5871910000-E	1550	Trenchless Installation of 16" Not In Soil	LF	35		
	SPU-1		Breakdown & Rebuild Manhole	EA	1		

Total \$130,184.00
OPINION OF PROBABLE COST

PROJECT: 17BP.8.R.19

CONTRACT:



VICINITY MAP
OFF-SITE DETOUR ROUTE

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

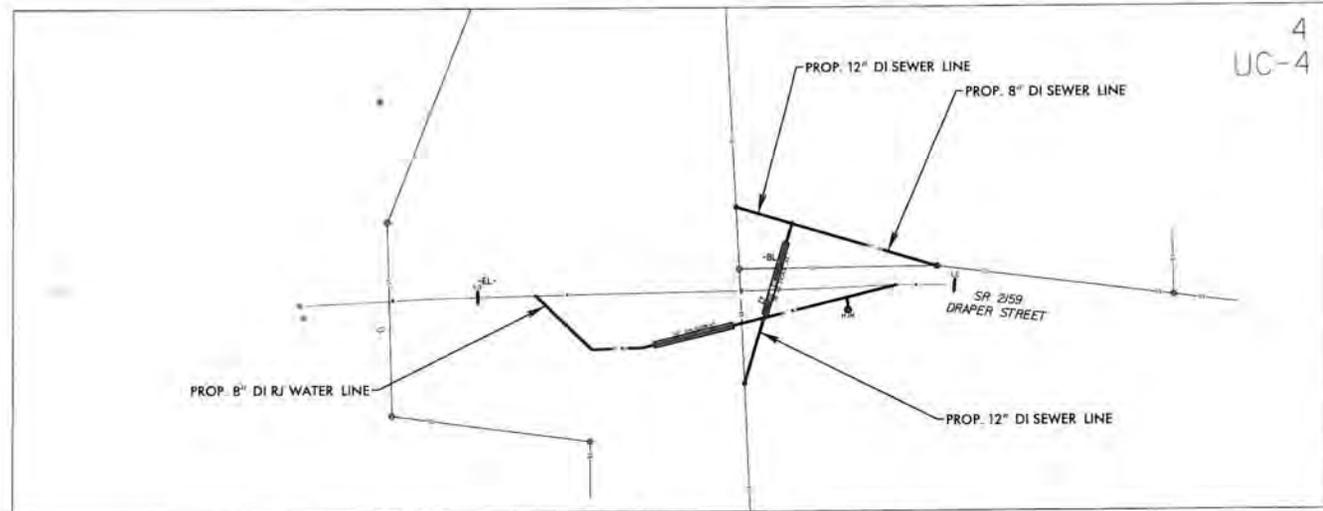
UTILITY CONSTRUCTION PLANS RANDOLPH COUNTY

T.I.P. NO. 17BP.8.R.19	SHEET NO. UC-1
----------------------------------	--------------------------

EXHIBIT C

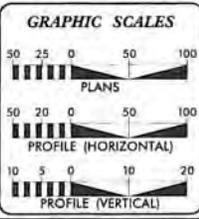
LOCATION: BRIDGE NO. 419 OVER PENWOOD BRANCH ON SR 2159 (DRAPER STREET)
TYPE OF WORK: UTILITIES

— CITY OF ASHEBORO COST
— NCDOT COST



LOCATION SKETCH

4
UC-4



INDEX OF SHEETS

SHEET NO.	DESCRIPTION
UC-1	TITLE SHEET
UC-2	UTILITY SYMBOLOGY SHEET
UC-3	GENERAL NOTES SHEET
UC-4	UTILITY CONSTRUCTION SHEET
UC-5	UTILITY PROFILE SHEET

WATER AND SEWER OWNERS ON PROJECT

(1) WATER - CITY OF ASHEBORO
(1) SEWER - CITY OF ASHEBORO

HINDE ENGINEERING
Lynchburg, VA
1201F Professional Blvd. Suite 210, Charlotte, NC 28217



STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

UTILITIES PLAN SHEET SYMBOLS

PROJECT REFERENCE NO.	SHEET NO.
17SP-B-A-19	UC-5
DESIGNED BY: ASB	
DRAWN BY: JNA	
CHECKED BY: KSN	
APPROVED BY:	
REVISED:	
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION UTILITIES ENGINEERING DIV. PHONE: (919) 707-6500 FAX: (919) 725-4151	
UTILITY CONSTRUCTION PLANS ONLY	

PROPOSED WATER SYMBOLS

Water Line (Sized as Shown)	
11 1/4 Degree Bend	
22 1/2 Degree Bend	
45 Degree Bend	
90 Degree Bend	
Plug	
Tee	
Cross	
Reducer	
Gate Valve	
Butterfly Valve	
Tapping Valve	
Line Stop	
Line Stop with Bypass	
Blow Off	
Fire Hydrant	
Relocate Fire Hydrant	
Remove Fire Hydrant	
Water Meter	
Relocate Water Meter	
Remove Water Meter	
Water Pump Station	
RPZ Backflow Preventer	
DCV Backflow Preventer	
Relocate RPZ Backflow Preventer	
Relocate DCV Backflow Preventer	

PROPOSED SEWER SYMBOLS

Gravity Sewer Line (Sized as Shown)	
Force Main Sewer Line (Sized as Shown)	
Manhole (Sized per Note)	
Sewer Pump Station	

PROPOSED MISCELLANEOUS UTILITIES SYMBOLS

Power Pole	
Telephone Pole	
Joint Use Pole	
Telephone Pedestal	
Utility Line by Others (Type as Shown)	
Trenchless Installation	
Encasement by Open Cut	
Encasement	

Thrust Block	
Air Release Valve	
Utility Vault	
Concrete Pier	
Steel Pier	
Plan Note	
Pay Item Note	

NOTE
PAY ITEM

EXISTING UTILITIES SYMBOLS

Power Pole	
Telephone Pole	
Joint Use Pole	
Utility Pole	
Utility Pole with Base	
H-Frame Pole	
Power Transmission Line Tower	
Water Manhole	
Power Manhole	
Telephone Manhole	
Sanitary Sewer Manhole	
Hand Hole for Cable	
Power Transformer	
Telephone Pedestal	
CATV Pedestal	
Gas Valve	
Gas Meter	
Located Miscellaneous Utility Object	
Abandoned According to Utility Records	AATUR
End of Information	E.O.I.

*Underground Power Line	
*Underground Telephone Cable	
*Underground Telephone Conduit	
*Underground Fiber Optics Telephone Cable	
*Underground TV Cable	
*Underground Fiber Optics TV Cable	
*Underground Gas Pipeline	
Aboveground Gas Pipeline	
*Underground Water Line	
Aboveground Water Line	
*Underground Gravity Sanitary Sewer Line	
Aboveground Gravity Sanitary Sewer Line	
*Underground SS Forced Main Line	
Underground Unknown Utility Line	
SUE Test Hole	
Water Meter	
Water Valve	
Fire Hydrant	
Sanitary Sewer Cleanout	

*For Existing Utilities
Utility Line Drawn from Record (Type as Shown)
Designated Utility Line (Type as Shown)

NORTH CAROLINA DEPT. OF TRANSPORTATION ENGINEERING DIVISION RALEIGH, NORTH CAROLINA				
BRIDGE NO. 419 OVER PENWOOD BRANCH OF SR 2150 (DRAPER STREET)				
Designed By	Checked By	Date	Scale	Sh.
KSH	KSN	6/27-13	UC-2	UC-5
 HINDE ENGINEERING <small>1001 W. HARRISVILLE RD. SUITE 200 CARRINGTON, NC 27513</small>				

UTILITY CONSTRUCTION

PROJECT REFERENCE NO. 1515-01-01	SHEET NO. UC-0
DESIGNED BY: KSH	
DRAWN BY: JMA	
CHECKED BY: KSH	
APPROVED BY:	
REVISED:	
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION	
UTILITIES ENGINEERING DIVISION PHONE: 919/217-1400 FAX: 919/217-1451	



GENERAL NOTES:

1. THE PROPOSED UTILITY CONSTRUCTION SHALL MEET THE APPLICABLE REQUIREMENTS OF THE NC DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" DATED JANUARY 2012.
2. THE EXISTING WATER AND SEWER FACILITIES BELONG TO CITY OF ASHEBORO.
3. ALL WATER LINES TO BE INSTALLED WITHIN COMPLIANCE OF THE RULES AND REGULATIONS OF THE NORTH CAROLINA DEPARTMENT OF ENVIRONMENTAL AND NATURAL RESOURCES, DIVISION OF ENVIRONMENTAL HEALTH, ALL SEWER LINES TO BE INSTALLED WITHIN COMPLIANCE OF THE RULES AND REGULATIONS OF THE NORTH CAROLINA DEPARTMENT OF ENVIRONMENT AND NATURAL RESOURCES, DIVISION OF WATER QUALITY. PERFORM ALL WORK IN ACCORDANCE WITH THE APPLICABLE PLUMBING CODES.
4. THE UTILITY OWNER OWNS THE EXISTING UTILITY FACILITIES AND WILL OWN THE NEW UTILITY FACILITIES AFTER ACCEPTANCE BY THE DEPARTMENT. THE DEPARTMENT OWNS THE CONSTRUCTION CONTRACT AND HAS ADMINISTRATIVE AUTHORITY. COMMUNICATIONS AND DECISIONS BETWEEN THE CONTRACTOR AND UTILITY OWNER ARE NOT BINDING UPON THE DEPARTMENT OR THIS CONTRACT UNLESS AUTHORIZED BY THE ENGINEER. AGREEMENTS BETWEEN THE UTILITY OWNER AND CONTRACTOR FOR THE WORK THAT IS NOT PART OF THIS CONTRACT OR IS SECONDARY TO THIS CONTRACT ARE ALLOWED, BUT ARE NOT BINDING UPON THE DEPARTMENT.
5. PROVIDE ACCESS FOR THE DEPARTMENT PERSONNEL AND THE OWNER'S REPRESENTATIVES TO ALL PHASES OF CONSTRUCTION. NOTIFY DEPARTMENT PERSONNEL AND THE UTILITY OWNER TWO WEEKS PRIOR TO COMMENCEMENT OF ANY WORK AND ONE WEEK PRIOR TO SERVICE INTERRUPTION. KEEP UTILITY OWNERS' REPRESENTATIVES INFORMED OF WORK PROGRESS AND PROVIDE OPPORTUNITY FOR INSPECTION OF CONSTRUCTION AND TESTING.

6. THE PLANS DEPICT THE BEST AVAILABLE INFORMATION FOR THE LOCATION, SIZE, AND TYPE OF MATERIAL FOR ALL EXISTING UTILITIES. MAKE INVESTIGATIONS FOR DETERMINING THE EXACT LOCATION, SIZE, AND TYPE MATERIAL OF THE EXISTING FACILITIES AS NECESSARY FOR THE CONSTRUCTION OF THE PROPOSED UTILITIES AND FOR AVOIDING DAMAGE TO EXISTING FACILITIES. REPAIR ANY DAMAGE INCURRED TO EXISTING FACILITIES TO THE ORIGINAL OR BETTER CONDITION AT NO ADDITIONAL COST TO THE DEPARTMENT.
7. MAKE FINAL CONNECTIONS OF THE NEW WORK TO THE EXISTING SYSTEM WHERE INDICATED ON THE PLANS, AS REQUIRED TO FIT THE ACTUAL CONDITIONS, OR AS DIRECTED.
8. MAKE CONNECTIONS BETWEEN EXISTING AND PROPOSED UTILITIES AT TIMES MOST CONVENIENT TO THE PUBLIC, WITHOUT ENDANGERING THE UTILITY SERVICE, AND IN ACCORDANCE WITH THE UTILITY OWNER'S REQUIREMENTS. MAKE CONNECTIONS ON WEEKENDS, AT NIGHT, AND ON HOLIDAYS IF NECESSARY.
9. ALL UTILITY MATERIALS SHALL BE APPROVED PRIOR TO DELIVERY TO THE PROJECT. SEE 1500-7, " SUBMITTALS AND RECORDS" IN SECTION 1500 OF THE STANDARD SPECIFICATIONS.

PROJECT SPECIFIC NOTES:

1. CONTRACTOR SHALL CONNECT ALL WATER LINES AND SERVICE CONNECTIONS USING NECESSARY FITTINGS.
2. CONTRACTOR SHALL COORDINATE ALL WORK WITH CITY OF ASHEBORO WATER AND SEWER INSPECTIONS. CONTACT NICK HYLTON AT (336) 626-1234 EXT. 2238.

UTILITY CONSTRUCTION

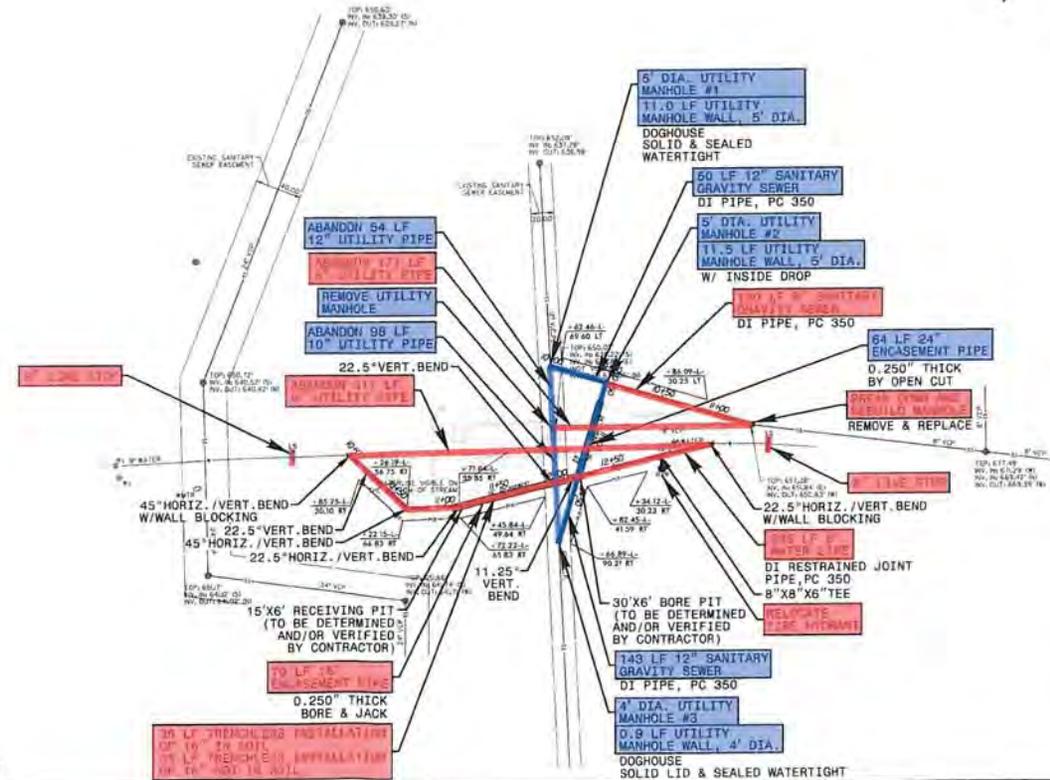
LIST OF STANDARD DRAWINGS

- 1515.01 WATER METER
- 1515.02 FIRE HYDRANT

NORTH CAROLINA DEPT. OF TRANSPORTATION ENGINEERING DIVISION RALEIGH, NORTH CAROLINA				
BRIDGE NO. 415 OVER HOWARD BRANCH ON SR 1152 CORNER STREET				
DRAWING NO.	DRAWING BY	DATE	SHEET	OF
1515	KSH	6/27/13	163	163
HINDE ENGINEERING <small>1001 FARMERSVILLE ROAD, SUITE 100, WELLSVILLE, NC 28688</small>				

PROJECT REFERENCE NO.	SHEET NO.
17BP.B.R.19	UC-4
DESIGNED BY: KSH	
DRAWN BY: JNA	
CHECKED BY: KSH	
APPROVED BY:	
REVISED:	
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION UTILITIES ENGINEERING SECTION PHONE: (919) 707-6690 FAX: (919) 250-4151	

UTILITY CONSTRUCTION



NOTE: SEE PLAN SHEET UC-5 FOR WATER & SEWER PROFILES

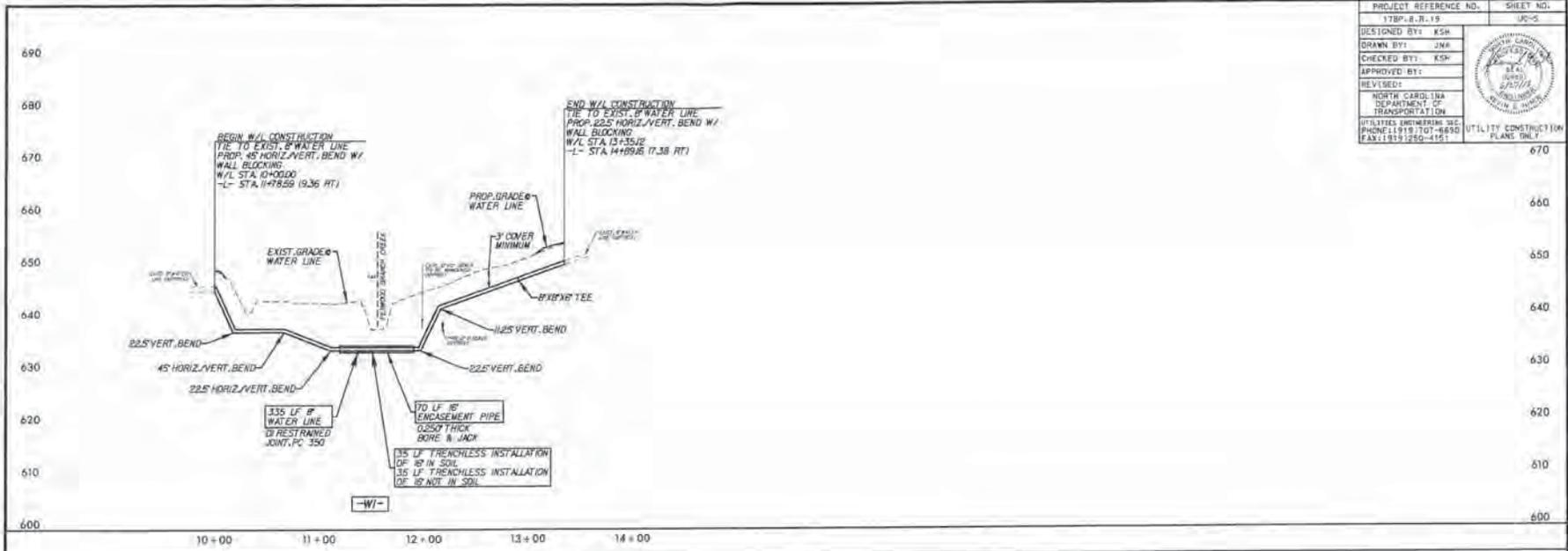
NORTH CAROLINA
DEPT. OF TRANSPORTATION
ENGINEERING DIVISION
WALTON, NORTH CAROLINA

BRIDGE NO. 419 OVER PEAWOOD BRANCH
ON SR 2159 (DRAPER STREET)

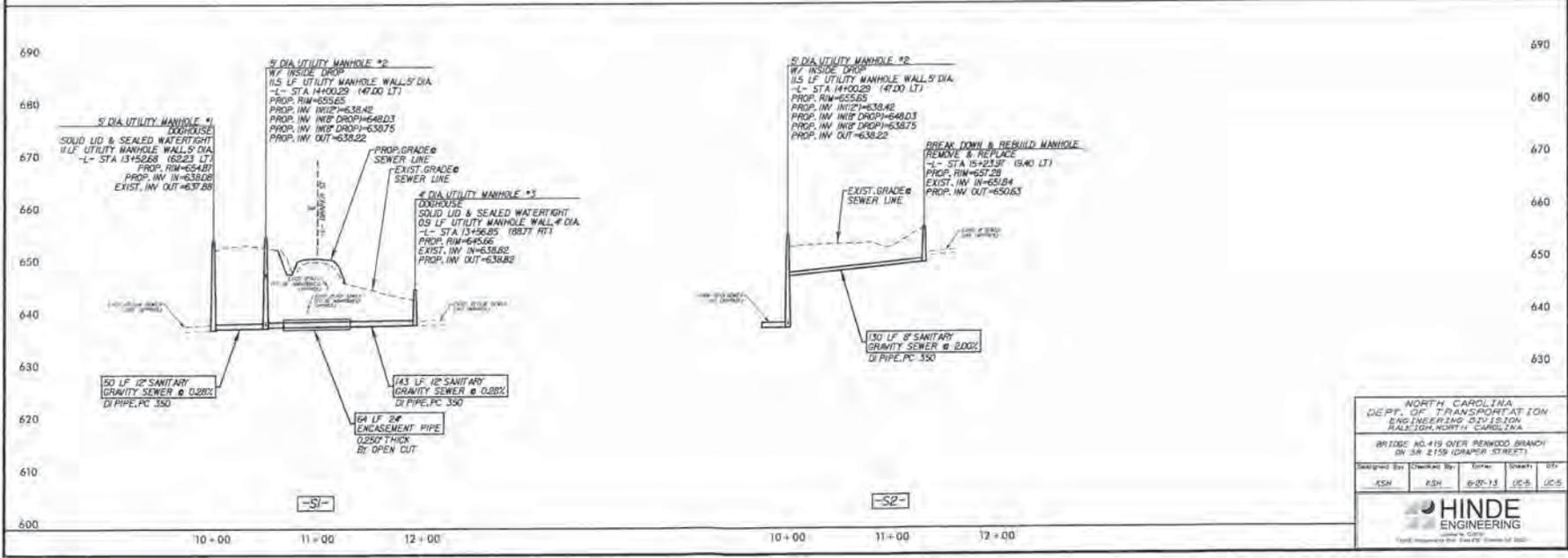
Designed By:	Checked By:	Date:	Sheet:	Of:
KSH	KSH	8-27-13	UC-4	UC-5

HINDE
ENGINEERING

1700 S. W. 10th St., Suite 202, Raleigh, NC 27603
Phone: (919) 876-8800 Fax: (919) 876-8801



PROJECT REFERENCE NO. 178P-B.R.19	SHEET NO. UC-5
DESIGNED BY: KSH	
DRAWN BY: JMK	
CHECKED BY: KSH	
APPROVED BY:	
REVIEWED:	UTILITY CONSTRUCTION PLANS ONLY
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION UTILITIES ENGINEERING INC. PHONE 1-919-707-6690 FAX 1-919-220-1151	

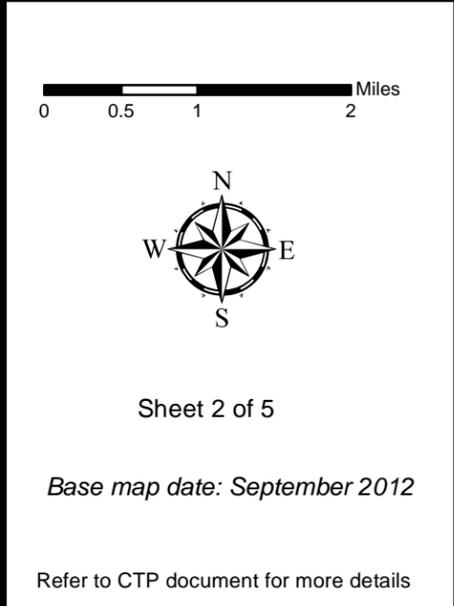
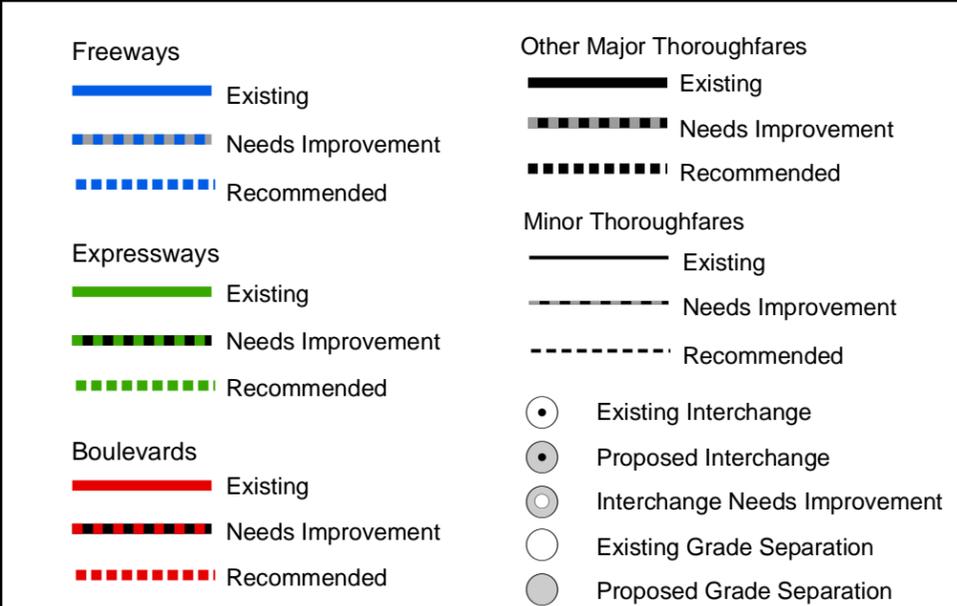
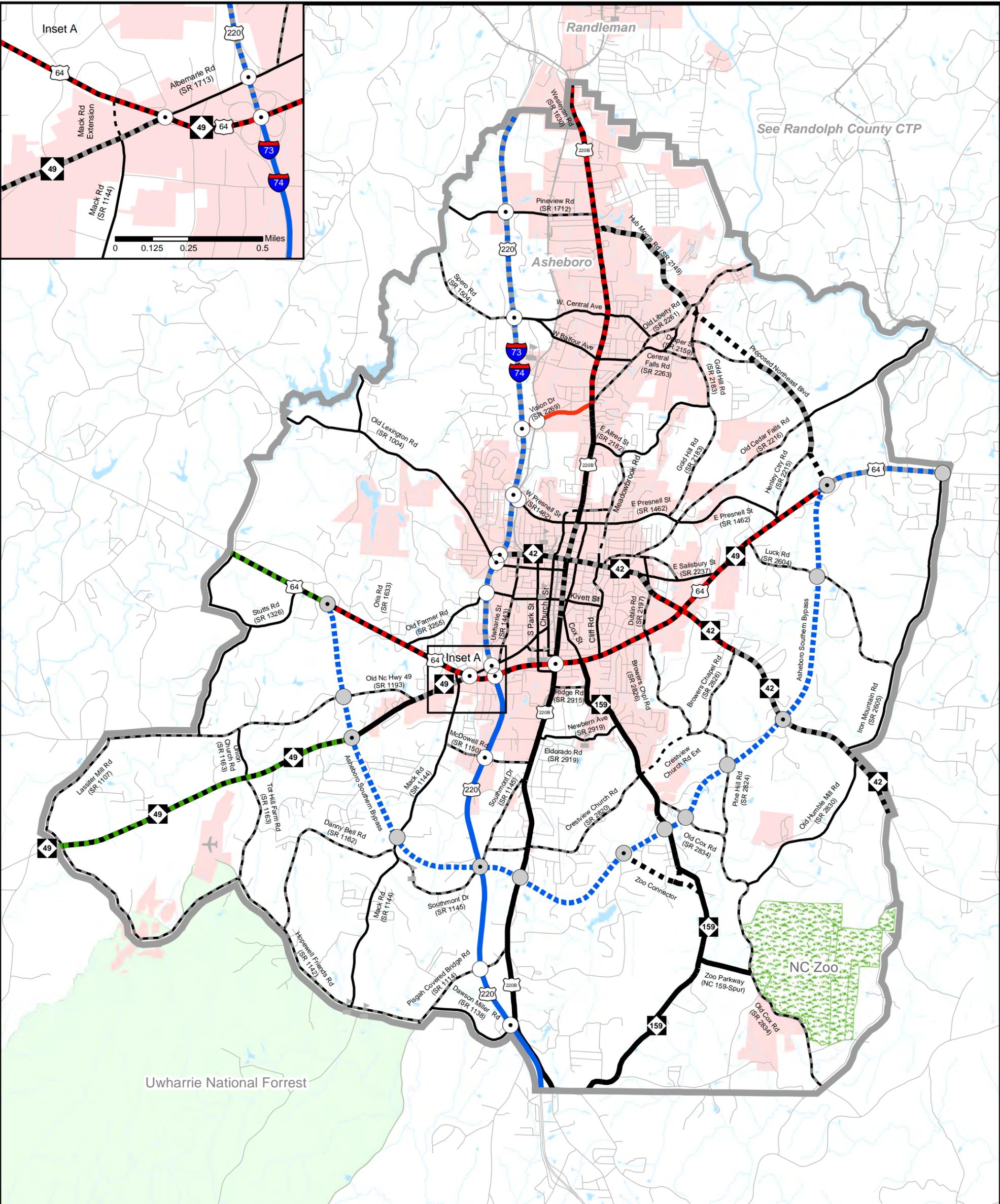


NORTH CAROLINA DEPT. OF TRANSPORTATION ENGINEERING DIVISION RALEIGH, NORTH CAROLINA BRIDGE NO. 419 OVER PENWOOD BRANCH ON SR. 2159 (GRAPER STREET)			
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Public Hearing concerning Asheboro Comprehensive Transportation Plan

Proposed maps and supporting documentation



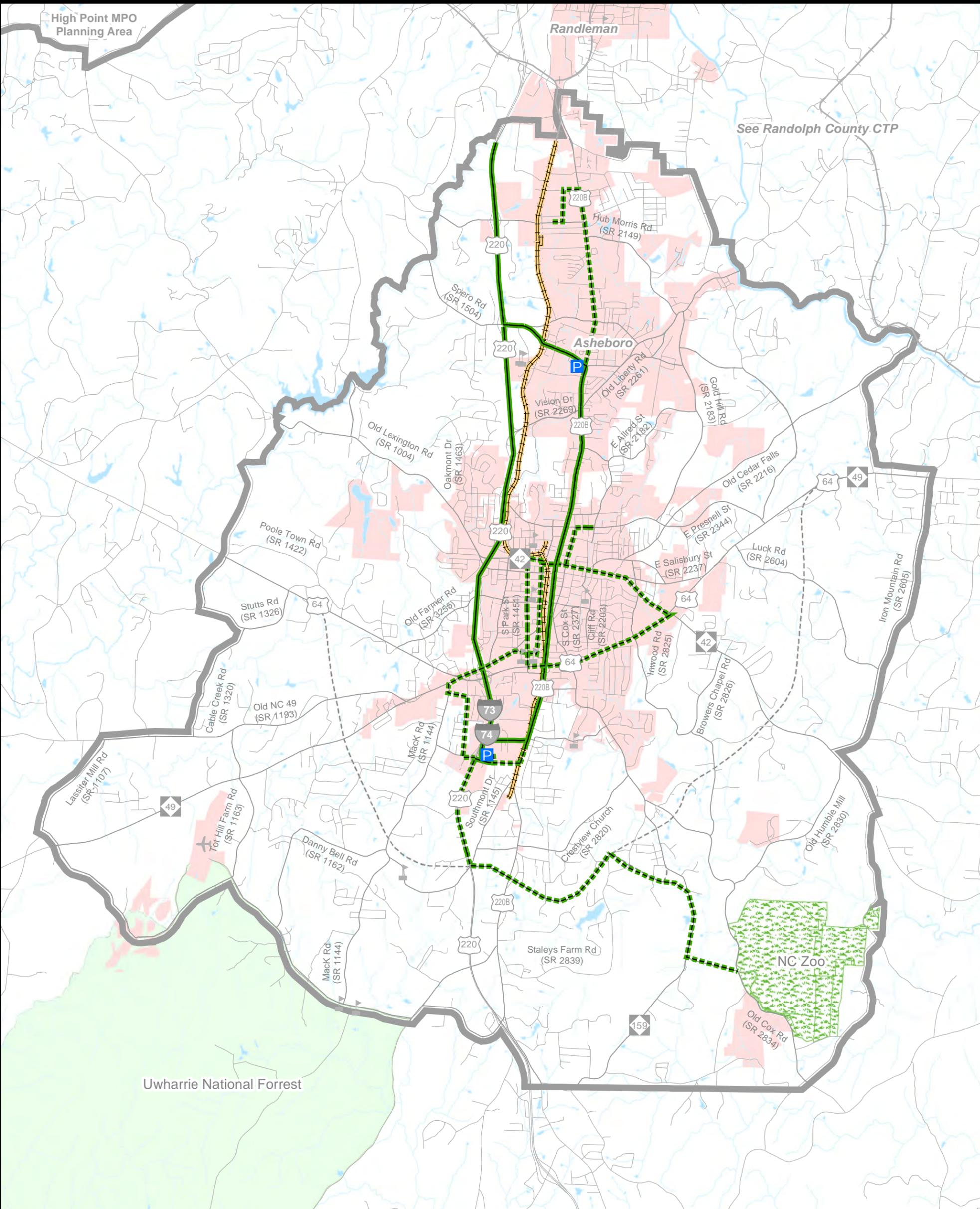
Highway Map

CITY OF
ASHEBORO
NORTH CAROLINA

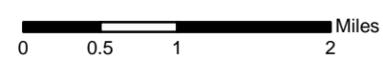
DRAFT

**Comprehensive
Transportation Plan**

Plan date: November 20, 2013



Bus Routes Existing Needs Improvement Recommended	Rail Corridor Active Inactive Recommended	Intermodal Connector Existing Recommended
Fixed Guideway Existing Needs Improvement Recommended	High Speed Rail Corridor Existing Recommended	Rail Stops Existing Recommended
Operational Strategies Existing Needs Improvement Recommended	Existing Grade Separation Proposed Grade Separation	Park and Ride Lot Existing Recommended



Sheet 3 of 5

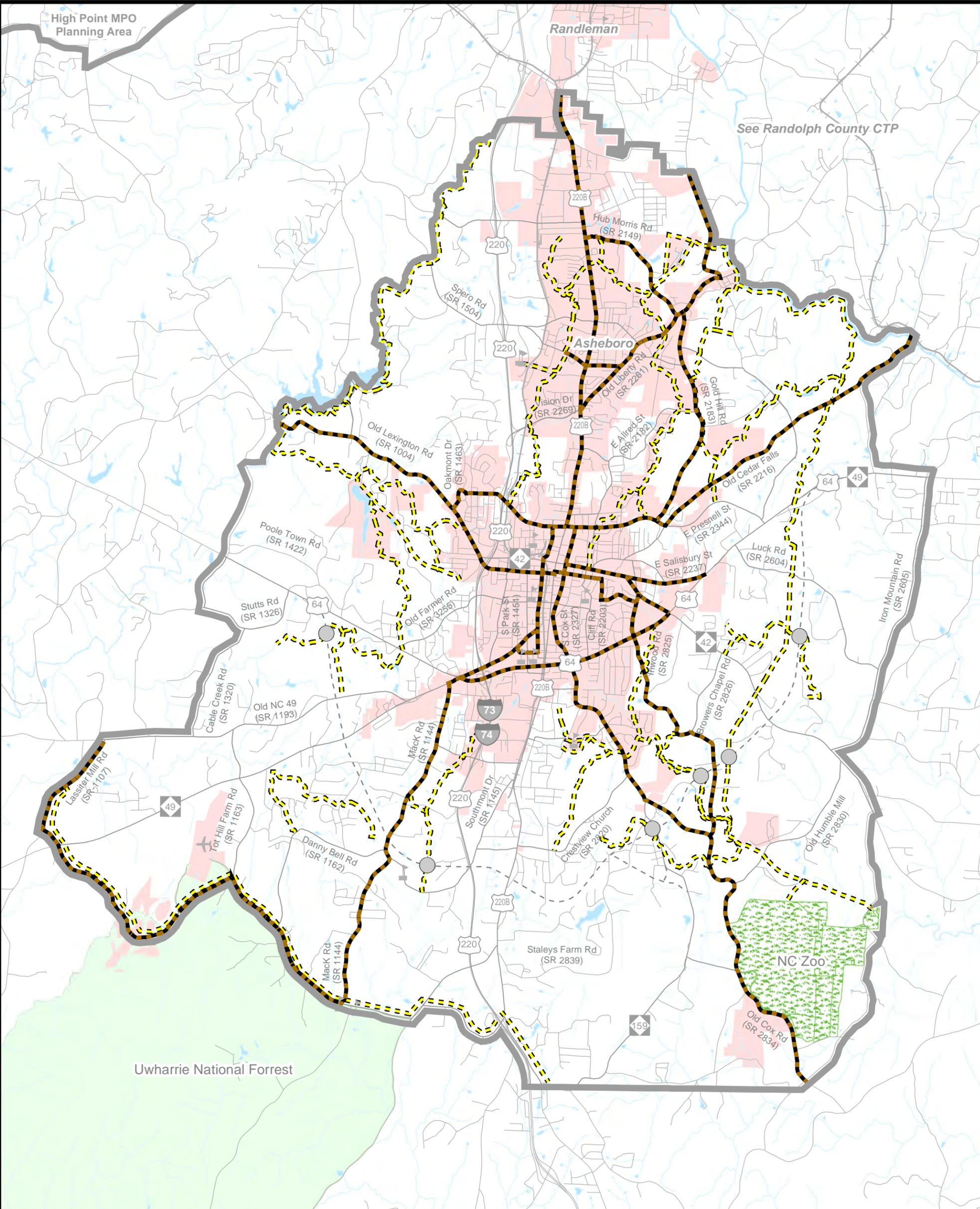
Base map date: September 2012

Refer to CTP document for more details

Public Transportation and Rail Map



DRAFT
Comprehensive Transportation Plan
 Plan date: November 20, 2013



On-road

- Existing
- Needs Improvement
- Recommended

Off-road

- Existing
- Needs Improvement
- Recommended

Multi-Use Paths

- Existing
- Needs Improvement
- Recommended

- Existing Grade Separation
- Proposed Grade Separation



Sheet 4 of 5

Base map date: September 2012

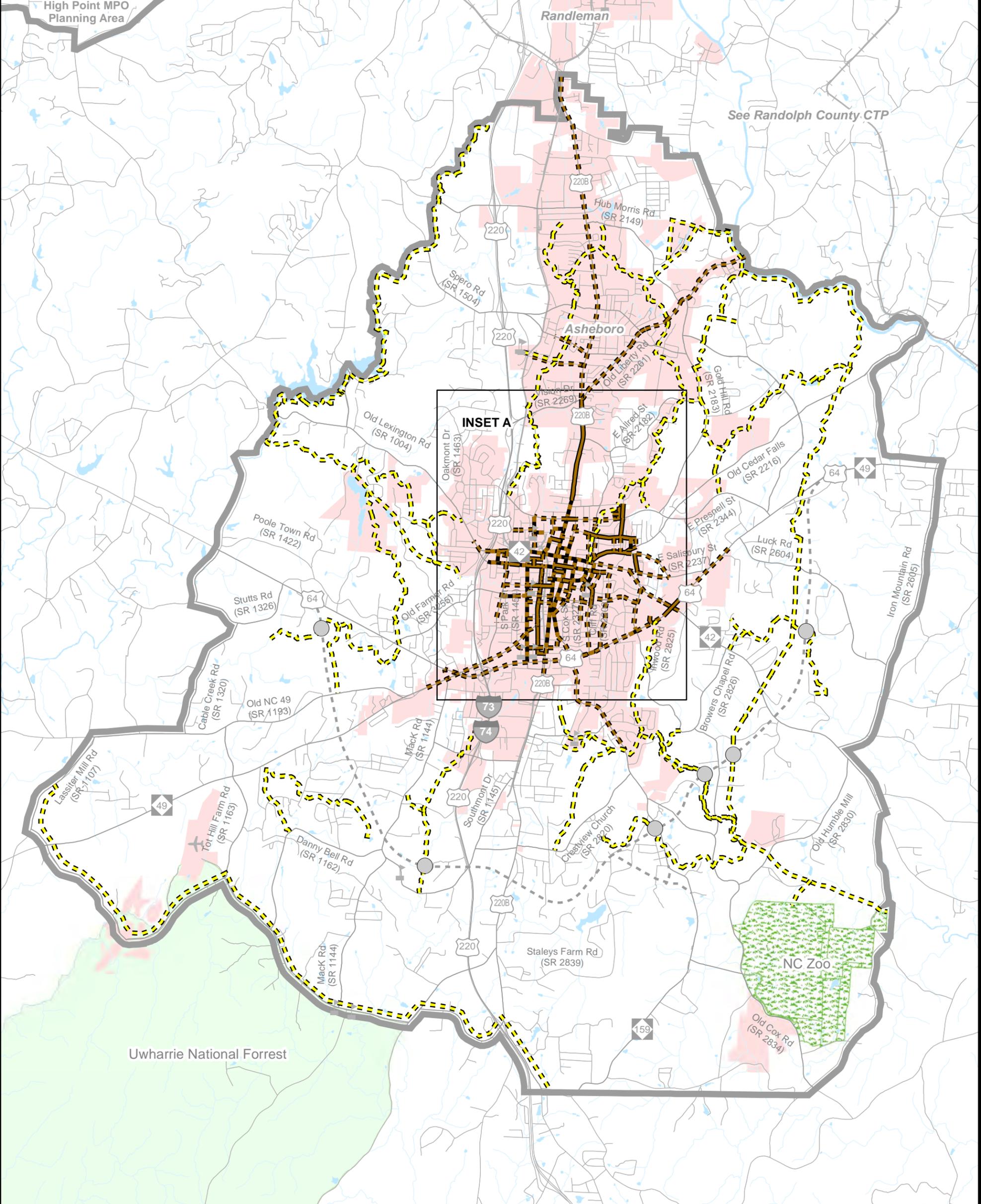
Refer to CTP document for more details

Bicycle Map



DRAFT

**Comprehensive
Transportation Plan**
Plan date: November 20, 2013



Sidewalks

- Existing
- Needs Improvement
- Recommended

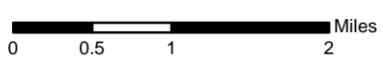
Off-Road

- Existing
- Needs Improvement
- Recommended

Multi-Use Paths

- Existing
- Needs Improvement
- Recommended

- Existing Grade Separation
- Proposed Grade Separation



Sheet 5 of 5

Base map date: September 2012

Refer to CTP document for more details

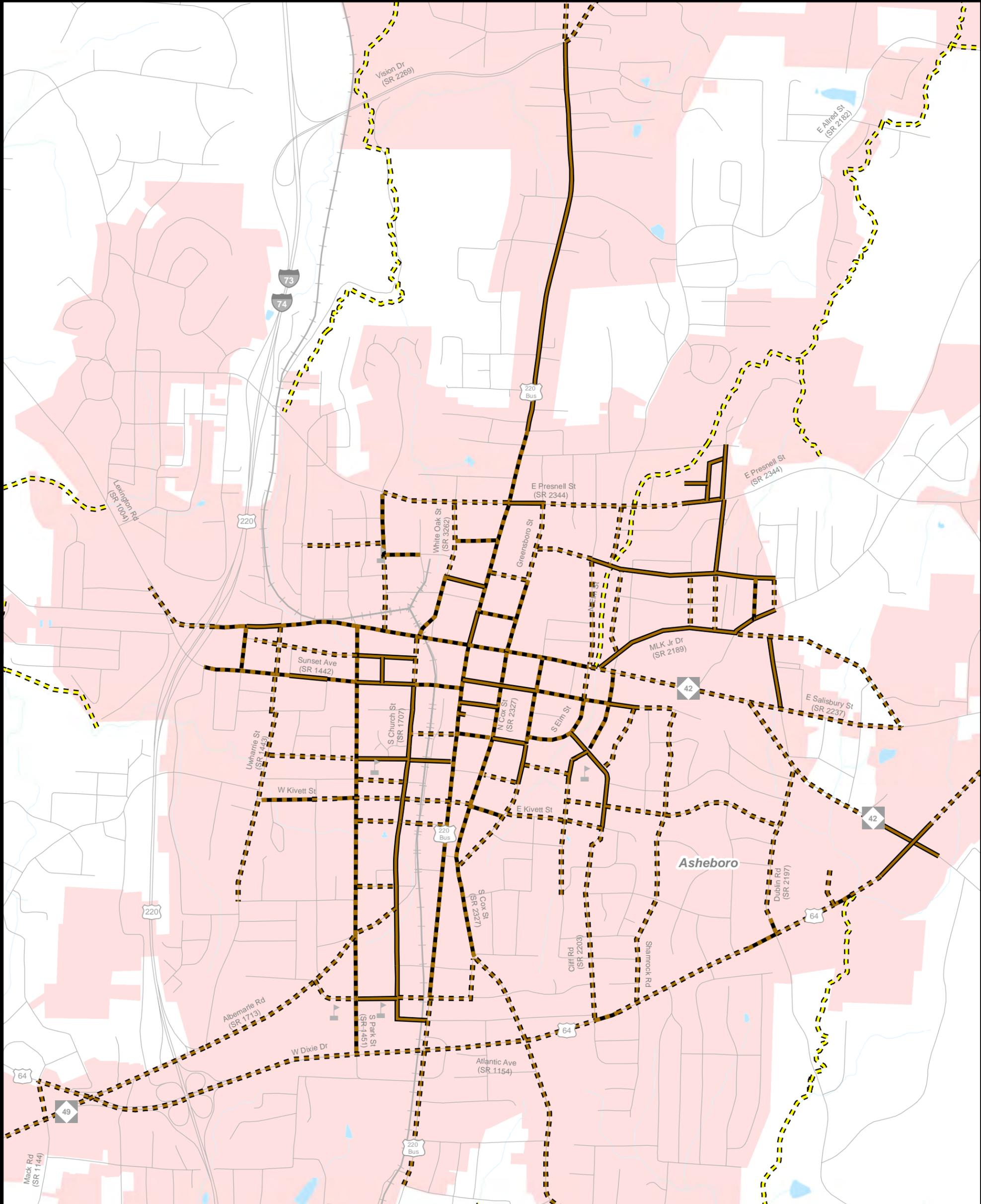
Pedestrian Map



DRAFT

Comprehensive Transportation Plan

Plan date: November 20, 2013



Sidewalks

-  Existing
-  Needs Improvement
-  Recommended

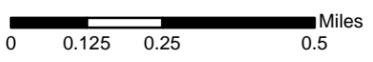
Off-Road

-  Existing
-  Needs Improvement
-  Recommended

Multi-Use Paths

-  Existing
-  Needs Improvement
-  Recommended

-  Existing Grade Separation
-  Proposed Grade Separation



Sheet 5A of 5

Base map date: September 2012

Refer to CTP document for more details

**Pedestrian Map
Inset A**



DRAFT

**Comprehensive
Transportation Plan**

Plan date: November 20, 2013

2. Recommendations

This chapter presents recommendations for each mode of transportation in the 2014 Asheboro CTP as shown in Figure 1. More detailed information on each recommendation is tabulated in Appendix C.

NCDOT adopted a "Complete Streets¹" policy in July 2009. The policy directs the Department to consider and incorporate several modes of transportation when building new projects or making improvements to existing infrastructure. Under this policy, the Department will collaborate with cities, towns and communities during the planning and design phases of projects. Together, they will decide how to provide the transportation options needed to serve the community and complement the context of the area. The benefits of this approach include:

- making it easier for travelers to get where they need to go;
- encouraging the use of alternative forms of transportation;
- building more sustainable communities;
- increasing connectivity between neighborhoods, streets, and transit systems;
- improving safety for pedestrians, cyclists, and motorists.

Complete streets are streets designed to be safe and comfortable for all users, including pedestrians, bicyclists, transit riders, motorists and individuals of all ages and capabilities. These streets generally include sidewalks, appropriate bicycle facilities, transit stops, right-sized street widths, context-based traffic speeds, and are well-integrated with surrounding land uses. The complete street policy and concepts were utilized in the development of the CTP. The CTP proposes projects that include multi-modal project recommendations as documented in the problem statements within this chapter. Refer to Appendix C for recommended cross sections for all project proposals and Appendix D for more detailed information on the typical cross sections.

2.1 Unaddressed Deficiencies

The following deficiencies were identified during the development of the CTP, but remain unaddressed.

US 220 Business (Fayetteville Street), Local ID: RAND0049-H

US 220 Business (Fayetteville Street) is currently over capacity from Presnell Street (SR1462) to NC 42 (Salisbury Street) and from Academy Street to Birkhead Drive. By 2040, these sections are projected to remain over capacity. Improvements are needed to relieve congestion on the existing facility such that a minimum of Level of Service (LOS) D can be achieved.

US 220 Business (Fayetteville Street) runs north-south through Asheboro and provides access to downtown Asheboro. This facility is a four lane major thoroughfare with 12 foot lanes from Presnell Street (SR1462) to NC 42 (Salisbury Street) and three lane major thoroughfare with 12 foot lanes and a center turn lane from Academy Street to

¹ For more information on Complete Streets, go to: <http://www.completestreetsnc.org/>

Birkhead Drive. Annual Average Daily Traffic (AADT) on US 220 Business between Presnell Street (SR 1462) and NC 42 (Salisbury Street) is projected to increase from 26,000 vehicles per day (vpd) in 2012 to 27,000 vpd in 2040, compared to a LOS D capacity of 23,500 vpd. Between Academy Street and Birkhead Drive, traffic is projected to increase in range from 15,000 to 16,000 vpd in 2012 to 16,000 to 17,000 vpd in 2040, compared to a LOS D capacity of 12,900 vpd.

These sections of US 220 Business (Fayetteville Street) are mostly strip development interspersed with residential and industrial development, a hospital and related services. There is no access control on these sections of US 220 Business (Fayetteville Street). It is lined with numerous driveway and roadway access points. A crash assessment performed during the development of the CTP identified numerous intersections and roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

The CTP project proposal (RAND0049-H) is to study and implement transportation demand management strategies along this corridor. The Piedmont Authority for Regional Transportation (PART) currently has an existing transit route along this facility and the Regional Coordinated Area Transportation System (RCATS) has proposed deviated fixed routes (Local ID: RAND003-T) connecting US 220 Business, US 64 and NC 42 to serve growing transportation needs. Strategies that promote other modes of transportation such as pedestrian, bicycle, transit and ridesharing are recommended for further study. Other strategies to be considered include access management, modifying signal timing, intersection improvements, driveway connections for businesses, service routes to the business for alternate access, and any other strategies to reduce turning conflicts and improve safety. Additionally, during the development of the CTP the City expressed the desire for intersection improvements to accommodate left turning traffic at US 220 Business (Fayetteville Street) and Presnell Street (SR 1462).

Based on the planning level environmental assessment using available GIS data, water and sewer pipes are located along this facility.

NC 42 (Salisbury Street), Local ID: RAND0050-H

NC 42 (Salisbury Street) is currently near or over capacity from Dublin Road (SR 2197) to I-73/74 (US 220 Bypass). By 2040, this section is projected to remain near or over capacity. Improvements are needed to relieve congestion on the existing facility such that a minimum of Level of Service (LOS) D can be achieved.

NC 42 provides access into downtown Asheboro and a direct connection to I-73/74 (US 220 Bypass). This facility is a two lane major thoroughfare with 12 foot lanes from Dublin Road (SR 2197) to Martin Luther King Jr. Drive (SR 2189), a three lane major thoroughfare with 12 foot lanes and a center turn lane from Martin Luther King Jr. Drive

(SR 2189) to Church Street (SR 1707), and a two lane major thoroughfare with 12 foot lanes from Church Street (SR 1707) to I-73/74 (US 220 Bypass). Annual Average Daily Traffic (AADT) on NC 42 (Salisbury Street) is projected to increase in range from 8,800 to 13,500 vehicles per day (vpd) in 2012 to 10,000 to 16,300 vpd in 2040, compared to a LOS D capacity of 11,600 to 13,600 vpd.

NC 42 (Salisbury Street) is mostly strip development interspersed with residential and industrial development. There is currently no access control on Salisbury Street. It is lined with numerous driveway and roadway access points, both residential and commercial. A crash assessment performed during the development of the CTP identified numerous intersections and roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

The CTP project proposal (RAND0050-H) is to study and implement transportation demand management strategies along this corridor. Strategies that promote other modes of transportation such as pedestrian, bicycle, transit and ridesharing are recommended for further study. Other strategies to be considered include access management, modifying signal timing, intersection improvements, driveway connections for businesses, service routes to the business for alternate access, and any other strategies to reduce turning conflicts and improve safety. Regional Coordinated Area Transportation System (RCATS) has a proposed deviated fixed route (Local ID: RAND003-T) connecting US 220 Business, US 64 and NC 42 that will help to serve the areas transportation needs. Additionally, during the development of the CTP the City expressed the desire for alignment improvements at the intersection of NC 42 and Dublin Road (SR 2197).

Based on the planning level environmental assessment using available GIS data, this section of NC 42 (Salisbury Street) is located in Cape Fear River Basin. There are also water and sewer pipes located along this facility. This facility also has a stream crossing just east of Elm Street.

2.2 Implementation

The CTP is based on the projected growth for the planning area. It is possible that actual growth patterns will differ from those logically anticipated. As a result, it may be necessary to accelerate or delay the implementation of some recommendations found within this plan. Some portions of the plan may require revisions in order to accommodate unexpected changes in development. Therefore, any changes made to one element of the CTP should be consistent with the other elements.

Initiative for implementing the CTP rests predominately with the policy boards and citizens of the city. As transportation needs throughout the state exceed available funding, it is imperative that the local planning area aggressively pursue funding for priority projects. Projects should be prioritized locally and submitted to the Piedmont

Triad RPO for regional prioritization and submittal to NCDOT. Refer to Appendix A for contact information on regional prioritization and funding. Local governments may use the CTP to guide development and protect corridors for the recommended projects. It is critical that NCDOT and local governments coordinate on relevant land development reviews and all transportation projects to ensure proper implementation of the CTP. Local governments and NCDOT share the responsibility for access management and the planning, design and construction of the recommended projects.

Prior to implementing projects from the CTP, additional analysis will be necessary to meet the National Environmental Policy Act (NEPA) or the North Carolina (or State) Environmental Policy Act² (SEPA). This CTP may be used to provide information in the NEPA/SEPA process.

2.3 Problem Statements

The following pages contain problem statements for each recommendation, organized by CTP modal element. The information provided in the problem statement is intended to help support decisions made in the NEPA/SEPA process. A full, minimum or reference problem statement is presented for each recommendation, with full problem statements occurring first in each section. Full problem statements are denoted by a gray shaded box containing project information. Minimum problem statements are more concise and less detailed than full problem statements, but include all known or readily available information. Reference problem statements are developed for TIP projects where the purpose and need for the project has already been established.

² For more information on SEPA, go to: <http://www.doa.nc.gov/clearing/faq.aspx>.

HIGHWAY

I-73/74/US 220 Proposed improvements from West Presnell Street (SR 1462) to 1.25 miles north of Pineview Road (SR 1712)

Local ID: RAND0068-H
Last updated: 11/20/2013

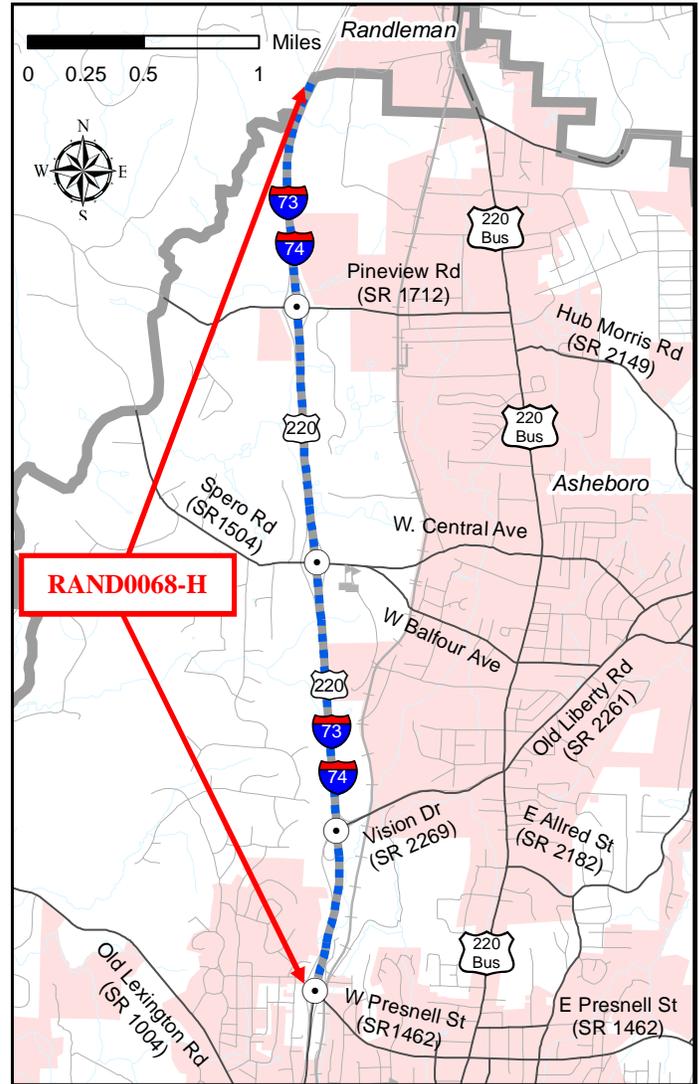
Identified Problem

I-73/74/US 220 is projected to be over capacity by 2040 from West Presnell Street (SR 1462) to 1.25 miles north of Pineview Road (SR 1712). Improvements are needed to accommodate projected traffic volumes and improve mobility through Asheboro such that a minimum Level of Service (LOS) D can be achieved.

Justification of Need

I-73/74/US 220 is a major north-south corridor in Randolph County, connecting Randleman and Asheboro with rural areas in the northern and southern parts of the county. The facility is a vital artery in moving people and goods through North Carolina, ultimately connecting North Carolina to South Carolina and Virginia.

I-73/74/US 220 is currently a four lane freeway with 12 foot lanes and is part of the statewide tier of the NC Multimodal Investment Network (NCMIN)³. Statewide tier facilities serve long-distance trips, connect regional centers, have the highest usage, and primarily serve mobility. I-73/74/US 220 is also part of the North Carolina Intrastate System and is intended to provide high-speed, safe travel service throughout the state.



By 2040, this facility is projected to be over capacity from 1.25 miles north of Pineview Road (SR 1712) to West Presnell Street (SR 1462) based on providing a LOS D. Traffic is projected to range from 34,000 to 36,000 vehicles per day (vpd) in 2012 to 60,000 to 64,000 vpd in 2040, compared to a LOS D capacity of 55,900 vpd.

³ For more information on NCMIN, go to: <http://www.ncdot.gov/performance/reform/NCMINmaps/>.

Community Vision and Problem History

Due to anticipated high traffic volumes in the future years, local officials have the desire to maintain the integrity of I-73/74; which is vital to the continued success of tourism for this area. This facility is the primary route used to access the NC Zoo, potteries in the Seagrove area south of Asheboro, and beaches in southern North Carolina and South Carolina.

The 2001 Asheboro Thoroughfare Plan identified this segment of road as deficient in the year 2025. The 2011 Randolph County Comprehensive Transportation Plan (CTP) also identified this segment of road as deficient in the year 2035.

CTP Project Proposal

Project Description and Overview

The proposed project (RAND0068-H) is to widen the existing facility from four lanes to six lanes and upgrade to interstate standards from West Presnell Street (SR 1462) to 1.25 miles north of Pineview Road (SR 1712).

A crash assessment performed during the development of the CTP identified two interchanges and four roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. This stretch of I-73/74/US 220 experienced 40 or more crashes during this time period. The interchanges at Spero Road (SR 1504) and W Presnell Street (SR 1462) experienced 10 to 19 crashes during the same period. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

The proposed improvements will help to reduce congestion along this facility. Additionally, it will fulfill the Strategic Highway Corridor⁴ vision, which recommends I-73/74 be upgraded to interstate standards.

Relationships to Land Use Plans

The Asheboro 2020 Land Development Plan⁵ (Updated October 2009) and the 2009 Randolph County Growth Management Plan⁶ recognize that Asheboro and Randolph County are anticipating an influx of both urban and suburban residential growth. The residential growth is anticipated to spread outwards from the core of Asheboro to the northern, western and eastern boundaries of Randolph County. A future interstate (I-73/I-74) along the current routing of US 220 and Asheboro's Southern Bypass (R-2536) will change land use patterns in the southern part of the county by attracting high intensity uses (retail and employment) at major intersections.

⁴ For more information on SHC, go to: <https://connect.ncdot.gov/projects/planning/Pages/StrategicHighwayCorridors.aspx>

⁵ To view this plan, go to: <http://www.ci.asheboro.nc.us/Forms%20&%20Documents/Planning/LDP%20with%20maps.pdf>

⁶ To view this plan, go to: <http://www.co.randolph.nc.us/downloads/2009GrowthManagementPlan.pdf>

Linkages to Other Plans and Proposed Project History

Interstates 73 and 74 were initially authorized by the Intermodal Surface Transportation and Efficiency Act (ISTEA) of 1991, as one of several high priority transportation corridors to be designated across the country. I-73/74/US 220 is designated as a freeway on NCDOT's Strategic Highway Corridor (SHC) Vision Plan.

This project directly connects to the proposed US 311 Bypass (R-2606) and to the I-73/74/US 220 improvements (I-4407) which include upgrading the facility to interstate standards. Improvements to this section of I-73/74/US 220 were identified in the 2001 Asheboro Thoroughfare Plan and 2011 Randolph County Comprehensive Transportation Plan.

Natural & Human Environmental Context

Based on a planning level environmental assessment using available GIS data, the portion of the proposed project north of Vision Drive (SR 2269) is within the water supply watershed area. This facility also has one deficient bridge; bridge 205 crosses West Presnell Street (SR 1462) and is classified as structurally deficient and functionally obsolete.

Multi-modal Considerations

The Piedmont Authority for Regional Transportation (PART) has an existing bus route from Guilford County to Randolph County, using the I-73/74 corridor. Additionally, PART has service enhancements that are being considered, which are directly connected to this project: US-311 Express, Park-n-Ride lot west of Randleman, and Asheboro Circular Routes/Local Transit Services. These enhancements were identified in the 2010 Regional Transit Development Plan⁷ (RTDP), which was developed by PART.

Public/ Stakeholder Involvement

No significant issues associated with this project were identified during the public/stakeholder involvement process.

⁷ For more information on the 2010 Regional Transit Development Plan, go to: <http://www.partnc.org/rtdp.html>

Identified Problem

I-73/74/US 220 in Asheboro is projected to be over capacity by 2040 from West Presnell Street (SR 1462) to US 64/NC 49 (Dixie Drive). Improvements are needed to relieve congestion on the existing facility such that a minimum Level of Service (LOS) D can be achieved.

Justification of Need

I-73/74/US 220 is a major north-south corridor in Randolph County, connecting Asheboro with rural areas in the northern and southern parts of the county. The facility is a vital artery in moving people and goods through North Carolina, ultimately connecting North Carolina to South Carolina and Virginia.

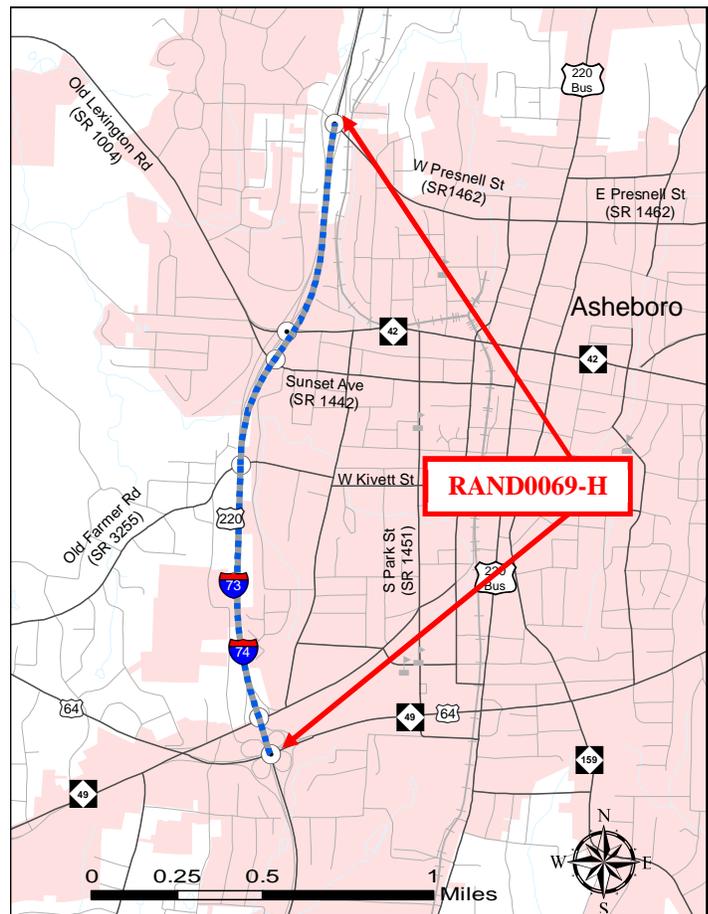
I-73/74/US 220 is currently a four lane freeway with 12 foot lanes and is part of the statewide tier of the NC Multimodal Investment Network (NCMIN). Statewide tier facilities serve long-distance trips, connect regional centers, have the highest usage, and primarily serve mobility. I-73/74/US 220 is also part of the North Carolina Intrastate System and is intended to provide high-speed, safe travel service throughout the state.

By 2040, this facility is projected to be over capacity from West Presnell Street (SR 1462) to US 64/NC 49 (Dixie Drive) based on providing a LOS D. Traffic is projected to increase from 34,000 vehicles per day (vpd) in 2009 to 62,000 vpd in 2040, compared to a LOS D capacity of 55,900 vpd.

Community Vision and Problem History

Due to anticipated high traffic volumes in the future years, local officials have the desire to maintain the integrity of I-73/74; which is vital to the continued success of tourism for this area. This facility is the primary route used to access the NC Zoo, potteries in the Seagrove area south of Asheboro, and beaches in southern North Carolina and South Carolina.

The 2001 Asheboro Thoroughfare Plan identified this segment of road as deficient in



the year 2025. The 2011 Randolph County Comprehensive Transportation Plan (CTP) also identified this segment of road as deficient in the year 2035.

CTP Project Proposal

Project Description and Overview

The CTP project proposal (Local ID: RAND0069-H) is to widen the existing facility from four to six lanes from West Presnell Street (SR 1462) to US 64/NC 49 (Dixie Drive).

A crash assessment performed during the development of the CTP identified three interchanges and two roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. This stretch of I-73/74/US 220 experienced 50 or more crashes during this time period. The following interchanges also experienced a high number of crashes during the same period:

- W Presnell Street (SR 1462) experienced 10 to 19 crashes;
- NC 42 experienced 30 to 39 crashes; and
- US 64 experienced 50 or more crashes.

The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

Relationships to Land Use Plans

The Asheboro 2020 Land Development Plan⁸ (Updated October 2009) and the 2009 Randolph County Growth Management Plan⁹ recognize that Asheboro and Randolph County are anticipating an influx of both urban and suburban residential growth. The residential growth is anticipated to spread outwards from the core of Asheboro to the northern, western and eastern boundaries of Randolph County. A future Interstate highway corridor (I-73/I-74) along the current routing of US 220 and Asheboro's Southern Bypass (R-2536) will change land use patterns in the southern part of the county by attracting high intensity uses (retail and employment) at major intersections.

Linkages to Other Plans and Proposed Project History

Interstates 73 and 74 were initially authorized by the Intermodal Surface Transportation and Efficiency Act (ISTEA) of 1991, as one of several high priority transportation corridors to be designated across the country. I-73/74/US 220 is designated as a freeway on NCDOT's Strategic Highway Corridor (SHC) Vision Plan.

The existing facility from West Presnell Street (SR 1462) to US 220 Bypass/NC 134 was recently upgraded to the interstate standards as part of TIP project I-4407.

This project directly connects to the proposed Asheboro Southern Bypass (R-2536) and to the I-73/74/US 220 improvements (TIP project I-4407).

⁸ To view this plan, go to: <http://www.ci.asheboro.nc.us/Forms%20&%20Documents/Planning/LDP%20with%20maps.pdf>.

⁹ To view this plan, go to: <http://www.co.randolph.nc.us/downloads/2009GrowthManagementPlan.pdf>.

Improvements to this section of I-73/74/US 220 were identified in the 2001 Asheboro Thoroughfare Plan and 2011 Randolph County Comprehensive Transportation Plan. The 2011 Randolph County Comprehensive Transportation Plan included the project proposal to widen the existing facility from four to six lanes from West Presnell Street (SR 1462) in Asheboro to NC 134/US 220 Business.

Natural & Human Environmental Context

Based on a planning level environmental assessment using available GIS data, the majority of the proposed project is within the local watershed area. The portion of the project south of Old Lexington Road (SR 1004) is within a natural heritage element occurrence area.

Multi-modal Considerations

The Piedmont Authority for Regional Transportation (PART) has an existing bus route from Guilford County to Randolph County, using the I-73/74 corridor. Additionally, PART has service enhancements that are being considered, which connect to this project: US-311 Express, Park-n-Ride Lot west of Randleman, and Asheboro Circular Routes/Local Transit Services. These enhancements were identified in the 2010 Regional Transit Development Plan¹⁰ (RTDP), which was developed by PART.

Public/ Stakeholder Involvement

No significant issues associated with this project were identified during the public/stakeholder involvement process.

¹⁰ For more information on the 2010 Regional Transit Development Plan, go to: <http://www.partnc.org/rtdp.html>

Identified Problem

US 220 Business is currently near capacity and is projected to be over capacity by 2040 from Claude Holden Drive (SR 2124) to Old Liberty Road (SR 2261). Improvements are needed to accommodate projected traffic volumes and improve mobility through Asheboro such that a minimum Level of Service (LOS) D can be achieved.

Justification of Need

US 220 Business is a major north-south corridor connecting Asheboro to Randleman and the rural areas in the northern and southern parts of the county.

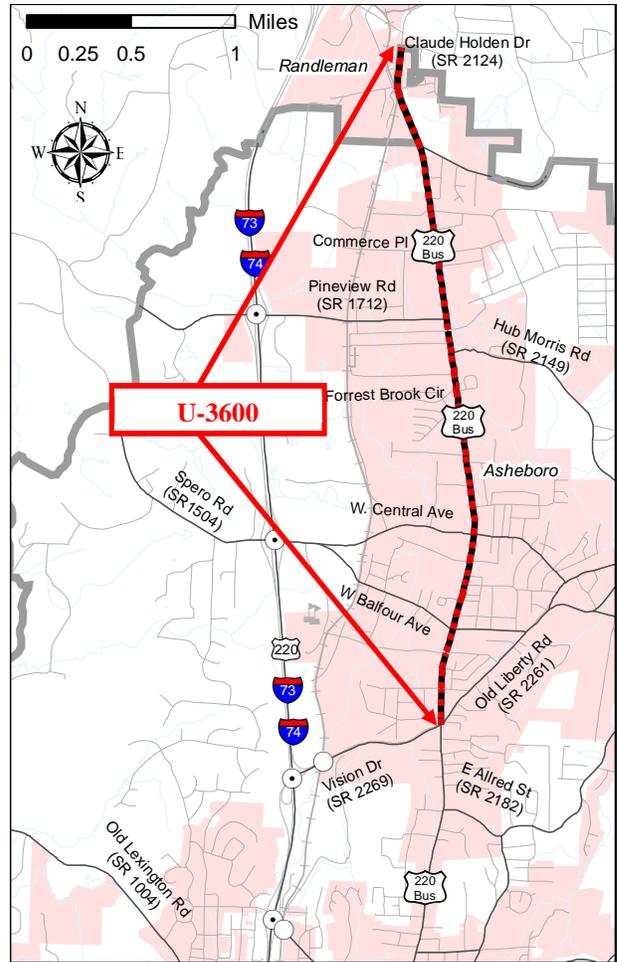
US 220 Business is two lane major thoroughfare with 12 foot lanes from Claude Holden Drive (SR 2124) to Pineview Road (SR 1712); a three lane major thoroughfare with a center left-turn lane and 12 foot lanes from Pineview Road (SR 1712) to Hub Morris Road (SR 2149) and a two lane major thoroughfare with 12 foot lanes from Hub Morris Road (SR 2149) to Old Liberty Road (SR 2261). It is part of the regional tier of the NC Multimodal Investment Network (NCMIN). Regional tier facilities connect major population centers and serve local land use.

By 2040, this facility is projected to be over capacity from Claude Holden Drive (SR 2124) to Old Liberty Road (SR 2261) based on providing a LOS D. Annual Average Daily Traffic (AADT) on US 220 Business is projected to increase in range from 11,000 to 12,000 vehicles per day (vpd) in 2012 to a range of 14,700 to 17,600 vpd in 2040, compared to a LOS D capacity of 12,700 to 13,800 vpd.

Community Vision and Problem History

US 220 Business provides access to downtown Asheboro and is also used to access the NC Zoo. US 220 Business has many driveways, both residential and commercial.

The 2001 Asheboro Thoroughfare Plan identified the segment of road from Forest Brook Circle to Old Liberty Road (SR 2261) as deficient in the year 2025. The 2011



Randolph County Comprehensive Transportation Plan (CTP) also identified this segment of road from Claude Holden Drive (SR 2124) to Old Liberty Road (SR 2261) as deficient in the year 2035.

CTP Project Proposal

Project Description and Overview

The proposed project (U-3600) is to widen the existing facility to a four lane boulevard from Claude Holden Drive (SR 2124) to Old Liberty Road (SR 2261). Sidewalks are recommended along the entire length of the project. Bicycle accommodations are also recommended along the entire length of the project. This project is not currently funded in the Transportation Improvement Program (TIP).

A crash assessment performed during the CTP identified numerous intersections and roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. Seven sections of US 220 Business experienced 4 to 9 crashes during this time period. The intersections at W Central Avenue (SR 1504) and Vision Drive (SR 2269) experienced 10 to 19 crashes during the same period. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

Relationships to Land Use Plans

Land use along US 220 Business is mainly residential, commercial and light industrial. The Asheboro 2020 Land Development Plan¹¹ (Updated October 2009) and the 2009 Randolph County Growth Management Plan¹² categorize this corridor into Municipal Growth Management Area. This growth area is contained within the corporate limits or extraterritorial planning & zoning jurisdictions of municipalities located within Randolph County. Mixed high-density urban growth is anticipated in these planning areas.

Linkages to Other Plans and Proposed Project History

This project directly connects to the proposed US 311 Bypass (R-2606) and to the I-73/74/US 220 improvements (I-4407) which include upgrading the facility to interstate standards. The 2001 Asheboro Thoroughfare Plan recommended widening the existing facility from two to five lanes with a center left turn lane from the US 311 Extension to Old Liberty Road (SR 2261). The 2011 Randolph County CTP recommended widening the existing facility from two to five lanes with a center left turn lane from the US 311 Extension to Old Liberty Road (SR 2261).

Natural & Human Environmental Context

Based on a planning level environmental assessment using available GIS data, there is a hazardous substance disposal site adjacent to the project in the southwest quadrant

¹¹ To view this plan, go to: <http://www.ci.asheboro.nc.us/Forms%20&%20Documents/Planning/LDP%20with%20maps.pdf>.

¹² To view this plan, go to: <http://www.co.randolph.nc.us/downloads/2009GrowthManagementPlan.pdf>.

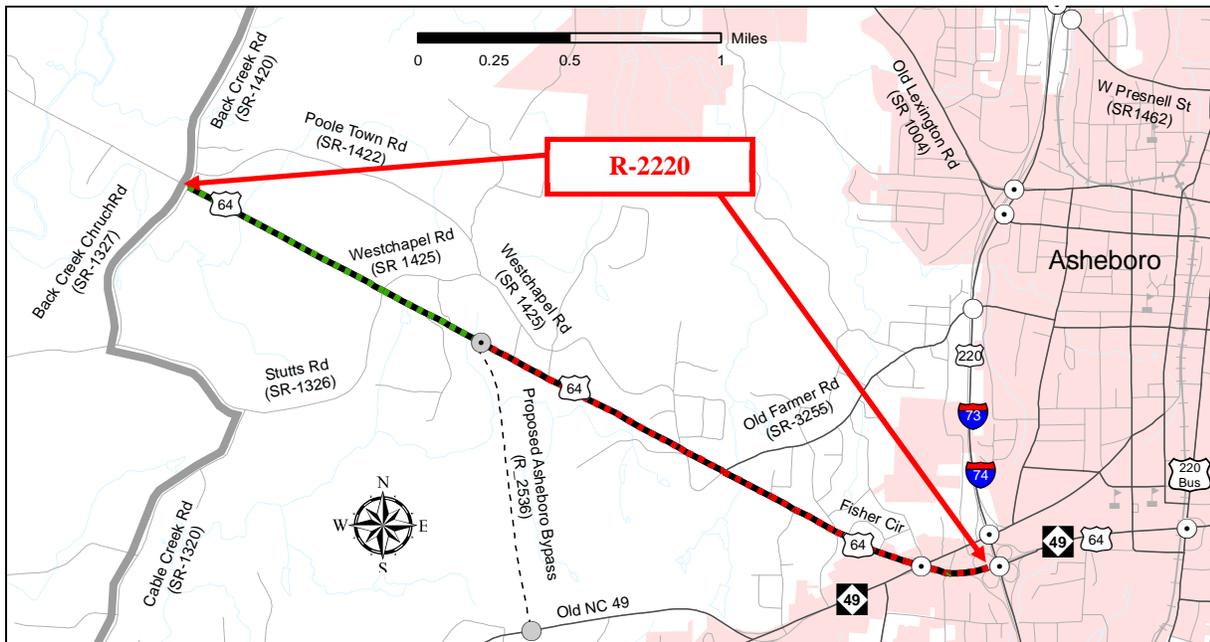
of the intersection of W Central Avenue. There are also water and sewer pipes along the proposed project.

Multi-modal Considerations

Sidewalks are recommended along the entire length of the proposed project. Bicycle accommodations are also recommended along the entire length of the project. The Piedmont Authority for Regional Transportation (PART) has an existing bus route from Guilford County to Randolph County using the section of US 220 Business. Additionally, Regional Coordinated Area Transportation System (RCATS) has service enhancements, including circulator routes, one of which will extend along US 220 BUS from W Balfour Avenue north to Commerce Place. Other circulator routes will also directly connect to US 220 Business.

Public/ Stakeholder Involvement

Respondents to the goal and objective survey conducted for the CTP identified traffic safety, truck traffic, and congestion along US 220 Business as major concerns. Additionally, US 220 Business was identified as desirable for providing pedestrian facilities.



Identified Problem

US 64 is projected to be near or over capacity by 2040 from western planning boundary at Back Creek Road (SR 1420) to I-73/74/US 220 Bypass. Improvements are needed to accommodate projected traffic volumes such that a minimum of Level of Service (LOS) D can be achieved.

Justification of Need

US 64, a major east-west corridor through Randolph County, connects the county seat of Asheboro with the eastern and western parts of the county. The US 64 corridor is vital to regional and statewide mobility and connectivity throughout the state. It is part of the statewide tier of the NC Multimodal Investment Network (NCMIN). Statewide tier facilities serve long-distance trips, connect regional centers, have the highest usage, and primarily serve mobility.

US 64 is currently a two lane facility, with 12 foot lanes from western planning boundary at Back Creek Road (SR 1420) to Westchapel Road (SR 1425); three lanes with 12 foot lanes and a center turn lane from Westchapel Road (SR 1425) to NC 49; and a four lane divided facility with 12 foot lanes from NC 49 to I-73/74/US 220 Bypass.

By 2040 the facility is projected to be near or over capacity based on the providing a LOS D. Traffic from western planning boundary at Back Creek Road (SR 1420) to I-73/74/US 220 Bypass is projected to increase in range from 11,000 to 12,000 vehicles per day (vpd) in 2012 to 14,000 to 16,000 vpd in 2040, compared to a LOS D capacity ranging from 14,600 to 15,900 vpd.

Community Vision and Problem History

US 64 is the primary route between Asheboro and Lexington, the county seats of Randolph County and Davidson County respectively. This roadway was first identified as an alternative to I-40/I-85 between the western part of the state and Raleigh in the US 64-NC 49 Corridor Study Report¹³. This study which was conducted by NCDOT Transportation Planning Branch in 2005, focused on the entire US 64-NC49 corridors between Charlotte, Statesville, and Raleigh. The study included extensive public involvement and stakeholder outreach, and ultimately provided a broad vision and strategy for the future of the corridor. This study only represents Phase 1 of the US 64-NC 49 improvements, which is primarily the vision for the corridor. Further study is intended to carry the project from vision to location specific implementation. As traffic reaches capacity on I-40, travelers look to US 64 as a viable alternative when traveling through central North Carolina.

This deficiency was identified in the 2011 Randolph County Comprehensive Transportation Plan.

CTP Project Proposal

Project Description and Overview

The CTP project proposal (R-2220) is to improve US 64 to an expressway from the western planning boundary at Back Creek Road (SR 1420) to the Asheboro Southern Bypass (R-2536) by widening the existing facility to four lanes with a median and improving US 64 to boulevard standards from the Asheboro Southern Bypass to I-73/74/US 220 Bypass. Sidewalks are recommended from I-73/I-74/US220 Bypass to Fisher Circle at the proposed Mack Road extension (U-5305). Bicycle accommodations are recommended from I-73/74/ US 220 Bypass to Albemarle Road (SR 1713). TIP Project R-2220 is not currently funded within the 2012 – 2018 Transportation Improvement Program.

A crash assessment performed during the development of the CTP identified numerous intersections and roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. Four sections of US 64 experienced 4 to 19 crashes during this time period. The interchange at the I-73/74/US 220 Bypass experienced 50 or more crashes during the same period. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

Relationship to Land Use Plans

The current land use along US 64 is mixed use development. It is heavily developed and consists of commercial and industrial uses right outside of and within the Asheboro city limits. The Asheboro 2020 Land Development Plan¹⁴ (Updated October 2009) and

¹³ For more information on the US 64-NC 49 Corridor Study, go to: <http://www.ncdot.gov/projects/us64phase1/>.

¹⁴ To view this plan, go to: <http://www.ci.asheboro.nc.us/Forms%20&%20Documents/Planning/LDP%20with%20maps.pdf>.

the 2009 Randolph County Growth Management Plan¹⁵ categorize this corridor into Secondary and Rural Growth Management Areas. These areas are likely to have water and sewer infrastructure in the foreseeable future or will predominately be mixed in use that will include residential, commercial and industrial developments. Implementation of the proposed Asheboro Southern Bypass will likely intensify these types of development west and south of Asheboro. From the city limits, transitional residential development is expected with major subdivisions scattered between agricultural and commercial land use patterns.

Linkages to Other Plans and Proposed Project History

This project directly connects with the Asheboro Southern Bypass (R-2536), the I-73/74 (I-4407) improvements, and the Mack Road (SR 1144) realignment (U-5305). The proposed improvements were also identified in the NCDOT's SHC Vision Plan to improve connectivity and mobility. The proposed project continues to the Davidson County line in 2011 Randolph County CTP. Additionally, the 2011 Davidson County CTP recommends the US 64 to be upgraded from a two lane major thoroughfare to a four lane expressway from Randolph County to I-85 in Lexington.

Natural & Human Environmental Context

Based on a planning level environmental assessment using available GIS data, the proposed project is within the water supply and watershed areas. The portion of the project east of the Asheboro Bypass to I-73/74/US 220 Bypass is within a natural heritage element occurrence area. It also crosses several streams, a geologic dike at Old Farmer Road (SR 3255) and a geologic fault near NC 49. Additionally, NCDOT's Structures Management Unit has identified bridge 171 as structurally deficient and functionally obsolete. This bridge is funded for replacement as TIP Project B-5363 in 2019.

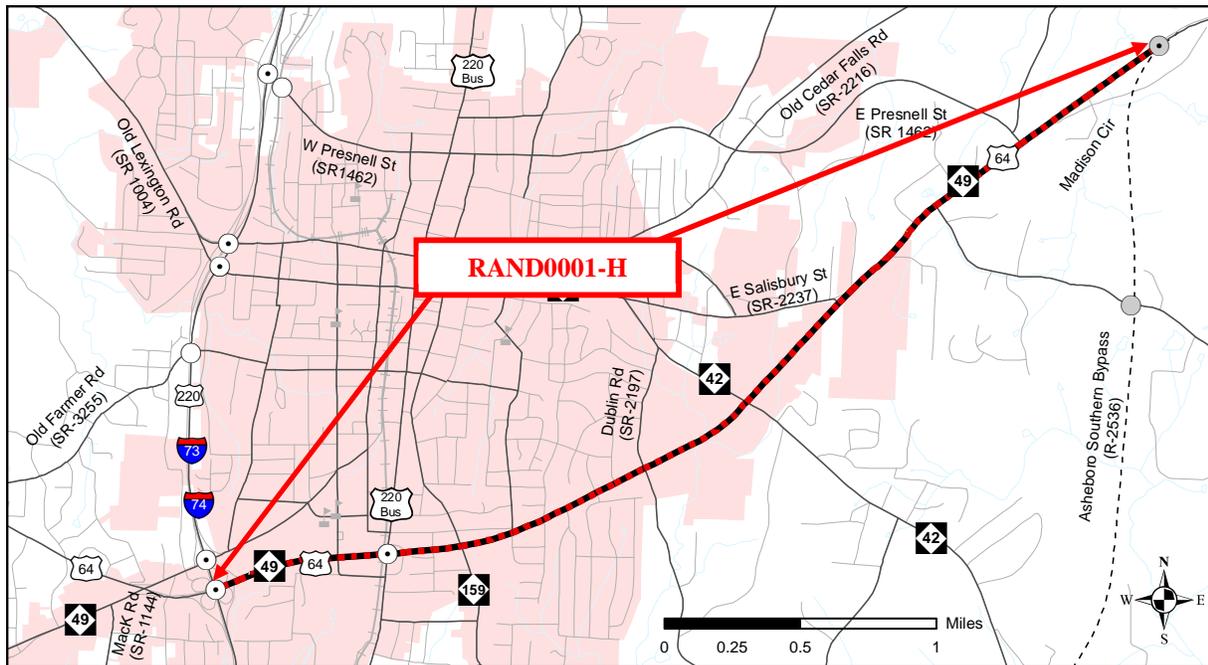
Multi-modal Considerations

There is a recommendation for a sidewalk on US 64 from I-73/I-74/US220 Bypass to Fisher Circle at the proposed Mack Road extension (U-5305). Bicycle accommodations are recommended from I-73/74/ US 220 Bypass to Albemarle Road (SR 1713).

Public/ Stakeholder Involvement

Improvements to US 64 were identified most frequently as a key transportation issue in the county by the respondents to the transportation survey conducted in conjunction with the Asheboro CTP study. Respondents ranked US 64 as their number one concern on the following issues: safety (particularly at intersections with other US and NC routes), truck traffic, congestion and access.

¹⁵ To view this plan, go to: <http://www.co.randolph.nc.us/downloads/2009GrowthManagementPlan.pdf>.



Identified Problem

US 64 (Dixie Drive) from I-73/74/US 220 Bypass to NC 42 is currently over capacity and is projected to remain over capacity by 2040. The primary purpose of this project is to relieve congestion on the existing facility and accommodate projected traffic volumes such that a minimum of Level of Service (LOS) D can be achieved.

Justification of Need

US 64, a major east-west corridor through Randolph County connects the county seat of Asheboro with the eastern and western parts of the county. This facility is a vital artery in connecting major cities and moving people and goods throughout central North Carolina. It is part of the statewide tier of the NC Multimodal Investment Network (NCMIN). Statewide tier facilities serve long-distance trips, connect regional centers, have the highest usage, and primarily serve mobility.

US 64 is a five lane major thoroughfare with 12 foot lanes from I-73/74/US 220 Bypass to the Asheboro Southern Bypass east of Madison Circle. Even with the implementation of the Asheboro Southern Bypass (R-2536), by 2040 the facility is projected to be over capacity based on the capacity of providing a LOS D. Traffic through Asheboro is projected to increase from 28,000 to 32,000 vpd in 2012, to 30,500 to 32,000 vpd in 2040, compared to a LOS D capacity of 27,600 vpd.

Community Vision and Problem History

US 64 is the primary east-west route through Asheboro and Randolph County. This corridor is a heavily strip developed facility. While much of the business activity in the

Asheboro area is located in its historic downtown along Fayetteville Street (US 220 Business) and Salisbury Street (NC 42), the majority of commercial businesses are located along this portion of the US 64 corridor. Given the total economic impact that these businesses provide to the community, it is clear that any improvements that take place along the corridor should also preserve and enhance its economic vitality.

US 64 was also identified as an alternative route to I-40 between the western part of the state and Raleigh in the 2005 US 64-NC 49 Corridor Study Report¹⁶. As traffic reaches capacity on I-40, travelers look to US 64 as a viable alternative when traveling through central North Carolina.

This deficiency was identified in the 2011 Randolph County Comprehensive Transportation plan.

CTP Project Proposal

Project Description

The CTP project proposal (Local ID: RAND0001-H) is to upgrade US 64 (Dixie Drive), from I-73/74/US 220 Bypass to the Asheboro Southern Bypass (R-2536) east of Madison Circle, to boulevard standards by converting the existing five lane facility into a four lane, median divided facility. Sidewalks are recommended along this facility from I-73/74/US 220 Bypass to the Randolph Mall (located immediately east of NC 42). Bicycle accommodations are recommended from NC 42 to I-73/74/ US 220 Bypass.

In addition to the roadway improvements, transportation demand management strategies are also recommended for this section of US 64 (Dixie Drive), which include strategies that promote other modes of transportation such as pedestrian, bicycle, transit and ridesharing. Other strategies to be considered include access management, modifying signal timing, intersection improvements, driveway connections for businesses, service routes to the business for alternate access, and any other strategies to reduce turning conflicts and improve safety.

The City is coordinating with NCDOT Division staff to seek potential solutions related to improving congestion at US 64/NC 49 (Dixie Drive) and NC 159 (Zoo Parkway). The feasibility of using Third Street and Atlantic Avenue or Ridge Street as the preferred route for inbound/outbound NC Zoo traffic to improve the traffic congestion at the intersection is under consideration.

A crash assessment performed during the CTP identified fourteen intersections and seventeen roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. The number of crashes along this stretch of US 64 ranged from 4 to 49 during this time period. Additionally, high crash intersections along this stretch of US 64 experienced from 10 to 50 or more crashes during the same period. The proposed improvements may reduce the amount

¹⁶ For more information on the US 64-NC 49 Corridor Study, go to: <http://www.ncdot.gov/projects/us64phase1/>.

and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

Relationship to Land Use Plans

This section of US 64 is heavily developed, consisting primarily of businesses, retail developments, service establishments and commercial enterprises. The Asheboro 2020 Land Development Plan¹⁷ (Updated October 2009) and the 2009 Randolph County Growth Management Plan¹⁸ indicate an influx of both urban and suburban residential growth in the project area. The residential growth is anticipated to spread outwards from the core of Asheboro to the northern, western, and eastern boundaries of Randolph County. The future interstate highway corridor (I-73/74) along the current routing of US 220 and the Asheboro Southern Bypass (TIP Project R-2536) will change land use patterns in the southern part of the county by attracting high intensity uses (retail and employment) at major intersections.

The US 64 project area falls into the Primary and Municipal Growth Management Areas, as identified in 2009 Randolph County Growth Management Plan and 2009 Asheboro 2020 Land Development Plan. Mixed high density urban growth that will include residential, commercial, and industrial development is anticipated in these areas.

Linkages to Other Plans and Proposed Project History

The proposed project directly connects with improvements for the Asheboro Southern Bypass (R-2536), US 64 (R-2220), and NC 49 (R-2535).

In 2005, NCDOT coordinated with the Piedmont Triad Rural Planning Organization (RPO) and the city of Asheboro to conduct a corridor study (US 64 – NC 49 Corridor Study) to evaluate the transportation, safety, mobility, and land use decisions between Raleigh and Charlotte.

In 2010-2011 NCDOT, in partnership with the city of Asheboro, Randolph County, and the Piedmont Triad RPO, did a more in depth study¹⁹ of US 64 – NC 49 from East Presnell Street (SR 1462) to I-73/74/US 220 Bypass. This study evaluated the purpose and functionality of the US 64 corridor and developed a concise set of recommendations that enhance mobility, improve safety, and preserve the economic vitality along the corridor.

This project was identified in the 2011 Randolph County Comprehensive Transportation Plan.

Natural & Human Environmental Context

Based on a planning level environmental assessment using available GIS data, the portion of this project between I-73/74/US 220 Bypass and NC 159 is within the local

¹⁷ To view this plan, go to: <http://www.ci.asheboro.nc.us/Forms%20&%20Documents/Planning/LDP%20with%20maps.pdf>.

¹⁸ To view this plan, go to: <http://www.co.randolph.nc.us/downloads/2009GrowthManagementPlan.pdf>.

¹⁹ To view the study report, go to: http://www.ncdot.gov/projects/us64phase1/download/US64_Asheboro_Final_Report.pdf.

watershed area. The section of the project immediately east of I-73/74/US 220 Bypass is within a natural heritage element occurrence area. There are also water and sewer lines along the proposed project. This facility also has one rail crossing which is grade separated. The rail line is operated by Norfolk Southern and the bridge (#173) is rated as functionally obsolete. Asheboro High School and South Asheboro Middle School are also adjacent to this facility at S Park Street.

Multi-modal Considerations

The 2010 Regional Transit Development Plan²⁰ produced by the Piedmont Authority for Regional Transportation (PART) includes providing fixed route bus services along this corridor from S Park Street (SR 1451) to NC 42. This is part of the proposed circulator route that would tie into existing service routes and will likely be implemented by RCATS pending available funding. Sidewalks are also recommended along this facility from I-73/74/US 220 Bypass to the east of Vista Parkway (SR 2706). Bicycle accommodations are recommended from NC 42 to I-73/74/ US 220 Bypass.

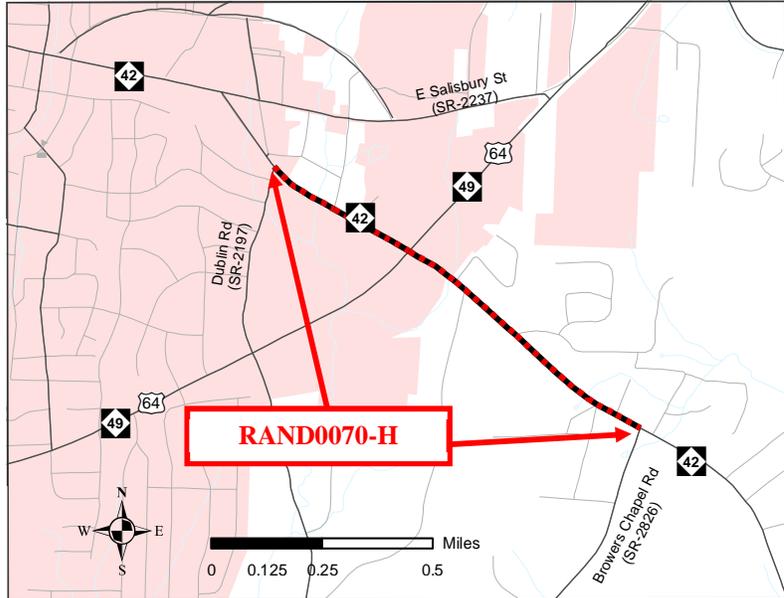
Public/ Stakeholder Involvement

Improvements to US 64 were identified most frequently as a key transportation issue in the county by the respondents to the transportation survey conducted in 2013 in conjunction with the CTP study. Respondents ranked US 64 as their number one concern on the following issues: safety (particularly at intersections with other US and NC routes), truck traffic, congestion and access. Additionally, US 64 was identified as desirable for providing bicycle and pedestrian facilities.

²⁰ For more information on the 2010 Regional Transit Development Plan, go to: <http://www.partnc.org/rtdp.html>

Identified Problem

NC 42 is currently near or over capacity and is projected to be over capacity by 2040 from Browsers Chapel Road (SR 2826) to Dublin Road (SR 2197). Improvements are needed to relieve congestion on the existing facility and accommodate projected traffic volumes such that a minimum of Level of Service (LOS) D can be achieved.



Justification of Need

NC 42, a major east-west corridor through Randolph County, connects the county seat of Asheboro with the eastern and western parts of the county. NC 42 provides access to downtown Asheboro. It is part of the regional tier of the NC Multimodal Investment Network (NCMIN). Regional tier facilities connect major population centers and serve local land use.

NC 42 is a two lane major thoroughfare with 10 foot lanes from Browsers Chapel Road (SR 2826) to Dublin Road. NC 42 has many driveways, both residential and commercial. Annual Average Daily Traffic (AADT) on this section of NC 42 is projected to increase from 13,000 vehicles per day (vpd) in 2012 to 13,600 to 14,200 vpd in 2040, compared to a LOS D capacity of 13,600.

Community Vision and Problem History

NC 42 is the major east-west route through Asheboro and Randolph County. This corridor is a heavily strip developed facility. Much of the business activity in the Asheboro area is located in its historic downtown along Fayetteville Street (US 220 Business) and Salisbury Street (NC 42). This is the first time this deficiency has been identified on a transportation plan.

CTP Project Proposal

Project Description

The project proposal (RAND0070-H) is to widen the existing facility to a four lane divided boulevard from Browsers Chapel Road (SR 2826) to Dublin Road (SR 2197). Sidewalks are recommended along NC 42 from Dublin Road (SR 2197) to the existing sidewalks north of US 64/ NC 49 (Dixie Drive). Bicycle accommodations are also recommended from Dublin Road (SR 2197) to US 64/ NC 49 (Dixie Drive). Transportation demand management strategies are also recommended in conjunction with the roadway improvements for this section of NC 42, which include strategies that

promote other modes of transportation such as pedestrian, bicycle, transit and ridesharing. Other strategies to be considered include access management, modifying signal timing, intersection improvements, driveway connections for businesses, service routes to the business for alternate access, and any other strategies to reduce turning conflicts and improve safety.

A crash assessment performed during the development of the CTP identified one intersection and several roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. Four sections of NC 42 experienced between 4 and 29 crashes during this time period. The intersection at US 64 experienced 50 or more crashes during the same period. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

Relationship to Land Use Plans

This section of NC 42 is heavily developed, consisting primarily of businesses and residential development. The Asheboro 2020 Land Development Plan²¹ (Updated October 2009) and the 2009 Randolph County Growth Management Plan²² categorize this corridor into Municipal Growth Management Area. This growth area is contained within the corporate limits or extraterritorial planning & zoning jurisdictions of municipalities located within Randolph County. Mixed high-density urban growth is anticipated in these planning areas.

Linkages to Other Plans and Proposed Project History

The proposed project directly connects with improvements for the Asheboro Southern Bypass (R-2536) and US 64 (R-2220). The 2011 Randolph County Comprehensive Transportation Plan recommended minor widening along this section of NC 42 to 12 foot lanes.

Natural & Human Environmental Context

Based on a planning level environmental assessment using available GIS data, the proposed project is located in Cape Fear River Basin. There are also water and sewer lines along the proposed project.

Multi-modal Considerations

Sidewalks are recommended along NC 42 from Dublin Road (SR 2197) to the existing sidewalks north of US 64/ NC 49 (Dixie Drive). Bicycle accommodations are also recommended from Dublin Road (SR 2197) to US 64/ NC 49 (Dixie Drive).

Public/ Stakeholder Involvement

Improvements to NC 42 were identified frequently as a key transportation issue in the county by the respondents to the transportation survey conducted in 2013 in

²¹ To view this plan, go to: <http://www.ci.asheboro.nc.us/Forms%20&%20Documents/Planning/LDP%20with%20maps.pdf>.

²² To view this plan, go to: <http://www.co.randolph.nc.us/downloads/2009GrowthManagementPlan.pdf>.

conjunction with the CTP study. Respondents ranked NC 42 high for safety issues. Additionally, NC 42 was identified as desirable for providing pedestrian facilities.

US 64 (Asheboro Southern Bypass), TIP No. R-2536

US 64 through Asheboro is projected to be over capacity by 2040. The 2012 – 2018 TIP includes project R-2536 that is intended to address this problem. This project includes constructing a four lane freeway on new location from US 64 west of Asheboro at Emerald Rock Road (SR 1325) to US 64 east of Asheboro near Madison Circle. This project is currently in the planning and design phase as a design-build project. For additional information about this project, including Purpose and Need, contact the NCDOT Project Development and Environmental Analysis Branch or visit the project website²³.

US 64, Local ID: RAND0003-H

US 64 from the Asheboro Southern Bypass east of Madison Circle to the eastern planning boundary at Iron Mountain Road (SR 2605) is projected to be near capacity by 2040. Improvements are needed to accommodate projected traffic volumes such that a minimum of Level of Service (LOS) D can be achieved.

US 64 is intended to serve mobility in Randolph County and, ultimately, connectivity within North Carolina and into Tennessee. US 64 is part of the Strategic Highway Corridor Vision (SHC) Plan adopted by NCDOT on September 2, 2004. Annual Average Daily Traffic (AADT) on US 64 is projected to increase in range from 17,000 to 19,000 vehicles per day (vpd) in 2012 to 26,000 to 28,000 vpd in 2040, compared to a LOS D capacity of 31,800.

Additionally, a crash assessment performed during the development of the CTP identified one intersection and four roadway sections along this corridor that experienced a high number of crashes between January 1, 2006 and December 31, 2010. Sections of US 64 experienced 4 to 19 crashes during this time period. The intersection at Trogon Pond Road (SR 2221) experienced 20 to 29 crashes during the same period. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

The project proposal is to upgrade the existing five lane facility to a four lane freeway from the Asheboro Southern Bypass (R-2536) east of Madison Circle to eastern planning boundary at Iron Mountain Road (SR 2605). A grade separation is recommended at Iron Mountain Road (SR 2605)/ Pleasant Cross Road (SR 2224).

NC 49, Local ID: R-2535

NC 49, from the western planning area boundary at Lassiter Mill Road (SR 1107) to the Asheboro Southern Bypass (R-2536) west of Old NC Hwy 49 (SR 1193), does not meet the future mobility needs in the central North Carolina.

NC 49 is intended to serve mobility in Randolph County and connectivity to the greater Charlotte region. NC 49 is part of the Strategic Highway Corridor Vision (SHC) Plan

²³ For more information on the Asheboro Southern Bypass, go to: <http://www.ncdot.gov/projects/asheboro/>.

adopted by NCDOT on September 2, 2004. This section of NC 49 is a two lane major thoroughfare with 12 foot lanes. A crash assessment performed during the development of the CTP identified four roadway sections along this corridor that experienced 4 to 19 between January 1, 2006 and December 31, 2010. The proposed improvements may reduce the amount and severity of crashes at these locations by removing the left turn conflicts. Refer to Chapter 1 of the CTP report for more detailed information on these locations.

The project proposal (R-2535) is to widen the existing facility to a four lane divided expressway from western planning boundary at Lassiter Mill Road (SR 1107) to the Asheboro Southern Bypass (R-2536) west of Old NC Hwy 49 (SR 1193). As development occurs along this corridor, every effort should be made to limit access in order to maintain mobility along the corridor.

Based on a planning level environmental assessment using available GIS data, the proposed project crosses several streams and two geologic faults west of Tot Hill Farm Road (SR 1163). Randolph County also identified recreational sites that are important to the area. The proposed project crosses one of those sites just west of Tot Hill Farm Road (SR 1163). The Asheboro Municipal Airport is located off of NC 49 along Tot Hill Farm Road (SR 1163).

This project recommendation was identified in the 2011 Randolph County Comprehensive Transportation Plan and continues into the county to Waynick Meadow Road (SR 1174) west of Farmer.

NC 49, TIP No. U-5305

NC 49 from Old Hwy 49 (SR 1193) to US 64 is projected to be near capacity by 2040. The 2012 – 2018 TIP includes project U-5305 that will address this deficiency.

TIP project U-5305 includes improving the intersection of NC 49 and Mack Road (SR 1144) by realigning and relocating the NC 49 western access ramp. The purpose of the project is to improve traffic flow, relieve congestion and increase the ability to carry projected traffic volumes on US 64, NC 49 and Mack Road. These improvements will also create safer intersections at Mack Road and US 64.

The U-5305 project is currently scheduled for construction in 2014. For additional information about this project, including the Purpose and Need, contact NCDOT's Project Development and Environmental Analysis Branch.

Elm Street/ Meadowbrook Road Extension, Local ID: RAND0073-H

US 220 Business (Fayetteville Street) is the primary north-south route in downtown Asheboro. By 2040, US 220 Business (Fayetteville Street) is projected to be over capacity based providing a LOS D capacity. Currently Elm Street and Meadowbrook Road serve as an alternative route to US 220 Business (Fayetteville Street) for residents in this area. Improvements are needed to help relieve congestion on US 220 Business (Fayetteville Street) and improve mobility within the downtown area.

Currently, from Elm Street motorist must make a right turn on Brewer Street and then turn left onto Meadowbrook Road in order to continue travelling north. The CTP project proposal includes realigning the intersections of Elm Street and Meadowbrook Road at Brewer Street to facilitate continuous north/south traffic movement in this residential area.

Based on a planning level environmental assessment using available GIS data, the proposed project may potentially impact water and sewer pipes. Additionally, there may potentially be impacts to residential property within the area.

This project recommendation was identified in the 2001 Asheboro Thoroughfare Plan and 2011 Randolph County Comprehensive Transportation Plan.

Crestview Church Road (SR 2820) Extension, Local ID: RAND0074-H

There are currently no contiguous east-west routes in southeastern section of the planning area. Improvements are needed to improve connectivity and mobility in this area.

US 64/NC 49 (Dixie Drive) is the primary east-west route through central Asheboro. It will continue to be a major carrier of traffic in Asheboro, and is projected to be over capacity by 2040 based on providing a LOS D. The CTP project proposal is to construct a two lane minor thoroughfare with 12 foot lanes to connect Crestview Church Road (SR 2820) to Browsers Chapel Road (SR 2826). While the proposed connector will be parallel to the Asheboro Southern Bypass (R-2220), it will serve an entirely different function. The proposed bypass will be a controlled access facility that will serve mobility but will not provide land access. The Crestview Church Road (SR 2820) Extension will provide access to this area, which is anticipated to develop over the next 20 to 25 years. The proposed extension will also provide an alternate route for residents in the southern section of the planning area to access US 64/NC 49 (Dixie Drive).

Based on a planning level environmental assessment using available GIS data, the proposed project will cross Vestal Creek.

This project recommendation was identified in the 2001 Asheboro Thoroughfare Plan and 2011 Randolph County Comprehensive Transportation Plan.

Northeast Boulevard, Local ID: RAND0071-H

There are currently no contiguous north-south routes in the northeastern section of the planning area. Improvements are needed to improve connectivity and mobility in this area. There is a need for contiguous facility from I-73/74 (US 220 Bypass) to US 64, east of city.

The primary route currently used for north-south travel is US 220 Business (Fayetteville Street) in central Asheboro. The primary routes currently used for east-west travel are

Presnell Street (SR 1462), NC 42 (Salisbury Street) and US 64 (Dixie Drive), which are all in central Asheboro. By 2040, US 220 Business (Fayetteville Street), NC 42 (Salisbury Street), and US 64 (Dixie Drive) are all projected to be over capacity based on the capacity of providing a LOS D. A lack of alternatives for contiguous travel between northern and eastern Asheboro exists. The northeastern portion of the planning area is one of the highest growth areas in Asheboro. As this area continues to develop, alternative routes to serve this area, particularly the commercial areas around the Randolph Mall on US 64, will be needed.

The project proposal is to create a continuous two lane major thoroughfare by:

- constructing a two lane connector with 12 foot lanes with paved shoulders on new location from US 64/NC 49 at the Asheboro Southern Bypass (R-2220) to Henley Country Road (SR 2215), 0.4 miles south of Old Cedar Falls Road (SR 2216);
- upgrading the existing Henley Country Road (SR 2215) to two 12 foot lanes with paved shoulders from the new connector, 0.4 miles south of Old Cedar Falls Road (SR 2216), to Giles Chapel Road (SR 2218);
- constructing a two lane connector with 12 foot lanes with paved shoulders on new location from Giles Chapel Road (SR 2218) to Old Liberty Road (SR 2261) at Hub Morris Road (SR 2149); and
- upgrading Hub Morris Road (SR 2149) to two 12 foot lanes with paved shoulders from Old Liberty Road (SR 2261) to US 220 Business. Bicycle accommodations are recommended along this facility.

Based on a planning level environmental assessment using available GIS data, the proposed project may potentially impact water and sewer pipes. It also crosses several streams and/or creeks. Additionally, the proposed project is adjacent to a landfill near Henley Country Road (SR 2215) at Randolph Tabernacle Road (SR 2217).

This project recommendation was identified in the 2001 Asheboro Thoroughfare Plan and 2011 Randolph County Comprehensive Transportation Plan.

Minor Widening Improvements

The following routes do not have capacity issues, but are recommended to be upgraded to 12 foot lanes with paved shoulders to improve mobility, safety and/or to accommodate bicycles.

- **NC 42, RAND0072-H:** from Browers Chapel Road (SR 2826) to Fairview Farm Road (SR 2831) at the southern planning boundary
- **Browers Chapel Road (SR 2826), RAND0016-H:** from US 64/NC 49 (E Dixie Drive) to NC 42
- **Cherry Street (SR 1443), RAND0064-H:** from NC 42 to Dixon Avenue (SR 1443)
- **Crestview Church Road (SR 2820), RAND0019-H:** from US 220 Business (Fayetteville Street) to NC 159
- **Danny Bell Road (SR 1162), RAND0020-H:** from Mack Road (SR 1144) Hopewell Friends Road (SR 1142)

- **Dixon Ave (SR 1443), RAND0051-H:** from S Cherry Street (SR 1443) to Uwharrie Street (SR 1443)
- **Draper Street (SR 2159), RAND0052-H:** from Central Farm Road (SR 2263) to Gold Hill Road (SR 2183)
- **Dublin Road (SR 2197), RAND0053-H:** from US 64/NC 49 (E Dixie Drive) to NC 42 (E Salisbury Street)
- **Giles Chapel Road (SR 2218), RAND0055-H:** from Gold Hill Road (SR 2183) to Henley Country Road (SR 2215)
- **Gold Hill Road (SR 2183), RAND0056-H:** from Old Liberty Road (SR 2261) to E Presnell Street (SR 1462)
- **Henley Country Road (SR 2215), RAND0025-H:** from 0.4 miles south of Old Cedar Falls Road (SR 2216) to E Presnell Street (SR 1462)
- **Hopewell Friends Road (SR 1142), RAND0057-H:** from Tot Hill Farms Road (SR 1163) to Pisgah Covered Bridge Road (SR 1114)
- **Lassiter Mill Road (SR 1107), RAND0061-H:** from Tot Hill Farm Road (SR 1163) to Old NC Highway 49 (SR 1193)
- **Luck Road (SR 2604), RAND0033-H:** from US 64/NC 49 (E Dixie Drive) to Iron Mountain Road (SR 2605)
- **Martin Luther King Jr. Drive (SR 2189), RAND0058-H:** from NC 42 (E Salisbury Street) to NC 42 (E Salisbury Street)
- **McDowell Road (SR 1150), RAND0059-H:** from Mack Road (SR 1144) to US 220 Business (Fayetteville Street)
- **Meadowbrook Road (SR 2184), RAND0060-H:** from Brewer Street (SR 1462) to E Allred Road Street (SR 2182)
- **Old Cedar Falls Road (SR 2216), RAND0062-H:** from Martin Luther King Jr. Drive (SR 2189) to Henley Country Road (SR 2215)
- **Old Cox Road (SR 2834), RAND0037-H:** from NC 159 to Fairview Farm Road (SR 2831) at the southern planning boundary
- **Old Liberty Road (SR 2261), RAND0038-H:** from US 220 Business (Fayetteville Street) to the Deep River at the eastern planning boundary. Bicycle accommodations are recommended from US 220 Business (Fayetteville Street) to Gold Hill Road (SR 2183)
- **Old NC Highway 49 (SR 1193), RAND0075-H:** from NC 49 to Lassiter Mill Road (SR 1107)
- **Pine Hill Road (SR 2824), RAND0044-H:** from Browers Chapel Road (SR 2826) to Old Cox Road (SR 2834)
- **Pineview Road (SR 1712), RAND0076-H:** from US 220 Business (Fayetteville Street) to 0.1 mile west of Sylvan Street
- **Pisgah Covered Bridge Road (SR 1114), RAND0045-H:** from the southern planning boundary at Hopewell Friends Road (SR 1142) to US 220 Business

(Fayetteville Street)

- **E Pritchard Street, RAND0054-H:** from US 220 Business (Fayetteville Street) to Meadowbrook Road (SR 2184)
- **Ridge Road (SR 2915), RAND0063-H:** from US 220 Business (Fayetteville Street) to NC 159
- **Southmont Drive (SR 1145), RAND0047-H:** from Mack Road (SR 1144) to US 220 Business (Fayetteville Street)
- **Spero Road (SR 1504), RAND0065-H:** from Back Creek at the western planning boundary to W Balfour Avenue (SR 1502)
- **Tot Hill Farm Road, RAND0066-H:** from Lassiter Mill Road (SR 1107) to NC 49
- **Uwharrie Street (SR 1443), RAND0067-H:** from W Kivett Street to Albemarle Road (SR 1713)

Other Local Initiatives

During the development of the CTP, the following local initiatives were also identified.

- **US 220 Business (S. Fayetteville Street):** The City expressed an interest in pursuing safety improvements on US 220 Business (S. Fayetteville Street) at Country Club Drive and Telephone Avenue.
- **Church Street:** The City expressed an interest in pursuing road diet and speed reduction on Church Street as a part of the countywide speed management action planning effort underway by the Piedmont Triad RPO.
- **Vision Drive:** On June 19, 2013, the Piedmont Triad RPO adopted the resolution supporting the City's desire for safety and access improvements along Vision Drive (SR 2269) near the I-734/74 interchange. The City is coordinating with NCDOT Division staff and improvements are currently under consideration.

PUBLIC TRANSPORTATION AND RAIL

The 2010 Regional Transit Development Plan²⁴ (RTDP), which was developed by PART, was used to identify existing and planned transit routes throughout the planning area. These facilities are shown on the Public Transportation and Rail Map, Sheet 3 of Figure 1. There are no rail projects proposed in this CTP.

BICYCLE

The 2003 Bicycling Randolph County map and the Central Park Bicycle Plan²⁵ were used to identify additional bicycle routes throughout the county. The Asheboro 2020 Land Development Plan (Updated October 2009) and 2011 Piedmont Triad Regional Trail Plan and Inventory (Final Report) were used to identify multi-use trails throughout the county. These facilities are shown on the Bicycle Map, Sheet 4 of Figure 1.

²⁴ For more information on the 2010 Regional Transit Development Plan, go to: <http://www.partnc.org/rtdp.html>.

²⁵ For more information on the Central Park Bicycle Plan, go to: <http://www.ptrc.org/index.aspx?page=221>.

Additionally, during the development of the CTP, the following facilities were recommended to have bicycle accommodations:

- **US 64/NC 49 (Dixie Drive), RAND0001-H:** from NC 42 to I-73/74/US 220 Bypass
- **US 64/NC 49 (Dixie Drive), R-2220:** from I-73/74/US 220 Bypass to Albemarle Road (SR 1713)
- **US 220 Business (Fayetteville Street), U-3600:** from the northern city limits to Old Liberty Road (SR 2261)
- **US 220 Business (Fayetteville Street), RAND0001-B:** from Old Liberty Road (SR2261) to East Presnell Street (SR 1462)
- **US 220 Business (Fayetteville Street), RAND0002-B:** from Worth Street to Sunset Avenue
- **NC 42, RAND0070-H:** from Dublin Road (SR 2197) to US 64/NC 49 (Dixie Drive)
- **NC 42 (Salisbury Street), RAND0050-H:** from Cox Street (SR 2327) to Dublin Road (SR 2197)
- **NC 49, RAND0013-B:** from US 64 to Mack Road (SR 1144)
- **Albemarle Road (SR 1713), RAND0003-B:** from US 64/NC 49 to Church Street
- **Church Street, RAND0004-B:** from W Walker Avenue to NC 42 (Salisbury Street)
- **Hub Morris Road (SR 2149), RAND0071-H:** from US 220 Business (Fayetteville Street) to Old Liberty Road (SR 2261)
- **Mack Road (SR 1144), RAND0005-B:** from US 64/NC 49 (Dixie Drive) to Hopewell Friends Road (SR 1142)
- **Old Liberty Road (SR 2261), RAND0038-H:** from US 220 Business (Fayetteville Street) to Gold Hill Road (SR 2183)
- **Park Drive (SR 1462), RAND0006-B:** from I-73/74/US 220 Bypass to Westmont Drive
- **Presnell Street (SR 1462), RAND0007-B:** from I-73/74/US 220 Bypass to Cox Street (SR 2327)
- **Salisbury Street (SR 2237), RAND0012-B:** from NC 42 to US 64/NC49 (Dixie Drive)
- **W Balfour Avenue, RAND0008-B:** from Rail Roads to Old Liberty Road (SR 2261)
- **W Walker Avenue, RAND0009-B:** from Albemarle Road (SR 1713) to Church Street
- **Westmont Drive, RAND0010-B:** from Park Drive (SR 1462) to Old Lexington Road (SR 1004)
- **White Oak Street (SR 3262), RAND0011-B:** from NC 42 (Salisbury Street) to Presnell Street (SR 2344)

PEDESTRIAN

The 2007 Sidewalk Inventory published by the Piedmont Triad Rural Planning Organization and 2008 City of Asheboro Comprehensive Pedestrian Transportation Plan identified recommended sidewalks for pedestrians throughout the county. In addition, the City is coordinating with NCDOT to pursue several safety related crosswalks in the downtown area.

The Asheboro 2020 Land Development Plan (Updated October 2009) and 2011 Piedmont Triad Regional Trail Plan and Inventory (Final Report) were used to identify multi-use trails throughout the county. These features are shown on the Pedestrian Map, Sheet 5 of Figure 1.

Additional facilities not included in the 2008 City of Asheboro Comprehensive Pedestrian Transportation Plan that are recommended to have sidewalks are listed below:

Sidewalks - Recommended (Sidewalks needed on both sides of a facility)

- **US 64, R-2220:** from NC 49 to proposed Mack Road Extension (U-5305)
- **US 64/NC 49 (Dixie Drive), R-2220:** from I-73/I-74/US220 Bypass to NC 49
- **US 64/NC 49 (Dixie Drive), RAND0001-H:** from 0.2 mile west of NC 42 to 0.1 mile east of Vista Parkway (SR 2706)
- **US 220 Business, U-3600:** from Old Liberty Road to Claude Holden Drive (SR 2124)
- **NC 49, RAND0001-P:** from US 64/NC 49 (Dixie Drive) to Oak Leaf Road (SR 1323)
- **NC 159 (Zoo Parkway), RAND0002-P:** from US 64/NC 49 (Dixie Drive) to 0.1 mile South of Brownmire Drive (SR 2941)
- **Albemarle Road (SR 1713), RAND0003-P:** from Uwharrie Street (SR 1445) to NC 49
- **City View Street (SR 1480), RAND0004-P:** from Summit Avenue to Peachtree Street
- **Cox Street (SR 2327), RAND0017-P:** from US 64/NC 49 (Dixie Drive) to Oakdale Street
- **East Kivett Street, RAND0005-P:** from South Main Street to Glenwood Road (SR 2203)
- **East Presnell Street (SR 2344), RAND0006-P:** from Greensboro Street to Vance Street.
- **Executive Way, RAND0007-P:** from US 64/NC 49 (Dixie Drive) to the Cul-de-sac
- **Greensboro Street, RAND0008-P:** from Brewer Street to East Presnell Street (SR 2344)
- **Lanier Avenue, RAND0009-P:** from Church Street (SR 1707) to US 220 Business

(Fayetteville Street).

- **Mack Road Extension (U-5305), RAND0010-P:** from US 64 to NC 49
- **Meadowbrook Road, RAND0011-P:** from East Presnell Street (SR 2344) to Brewer Street
- **North Elm Street, RAND0012-P:** from Salisbury Street (SR 2237) to Worth Street
- **Old Liberty Road (SR 2261), RAND0038-H:** from US 220 Business to 0.1 mile east of Little Point Road (SR 2145)
- **West Bailey Street, RAND0013-P:** from North Fayetteville Street (US 220 Business) to North Asheboro School Road (SR 1723)
- **West Balfour Avenue (SR 1502), RAND0014-P:** from US 220 Business to Burmil Road
- **West Walker Avenue, RAND0015-P:** from South Park Street (SR 1451) to Albemarle Road (SR 1713)
- **Worth Street, RAND0016-P:** from Shamrock Road to South High Street.

Sidewalks- Needs Improvement (Sidewalks needed on one side of a facility)

- **Foust Street, RAND0018-P:** from Church Street (SR 1707) to Ross Street
- **Miller Street, RAND0019-P:** from White Oak Street (SR 3262) to US 220 Business (Fayetteville Street)
- **Ross Street, RAND0020-P:** from Presnell Street (SR 2237) to Foust Street

Appendix C

CTP Inventory and Recommendations

Assumptions/ Notes:

- ❖ **Local ID:** This Local ID is the same as the one used for the Prioritization Project Submittal Tool. If a TIP project number exists it is listed as the ID. Otherwise, the following system is used to create a code for each recommended improvement: the first 4 letters of the county name is combined with a 4 digit unique numerical code followed by '-H' for highway, '-T' for public transportation, '-R' for rail, '-B' for bicycle, '-M' for multi-use paths, or '-P' for pedestrian modes. If a different code is used along a route it indicates separate projects will probably be requested. Also, upper case alphabetic characters (i.e. 'A', 'B', or 'C') are included after the numeric portion of the code if it is anticipated that project segmentation or phasing will be recommended.
- ❖ **Jurisdiction:** Jurisdictions listed are based on municipal limits, county boundaries, and MPO Metropolitan Planning Area Boundaries (MAB), as applicable.
- ❖ **Existing Cross-Section:** Listed under 'Total Width (ft)' is the approximate width of the roadway from edge of pavement to edge of pavement and under 'Lane Width (ft)' is the approximate width of a single lane based on centerline/ edge line markings. Listed under 'Lanes' is the total number of lanes, with 'D' if the facility is divided, and 'OW' if it is a one-way facility.
- ❖ **Existing ROW:** The estimated existing right-of-way is based on Road Characteristics Layer. These right-of-way amounts are approximate and may vary.
- ❖ **Existing and Proposed Capacity.** The estimated capacities are given in vehicles per day (vpd) based on LOS D for existing facilities and LOS C for new facilities. These capacity estimates were developed based on the 2000 Highway Capacity Manual using the Transportation Planning Branch's LOS D Standards for Systems Level Planning, as documented in Chapter 1.
- ❖ **Existing and Proposed Volumes,** given in vehicles per day (vpd), are estimates only based on a systems-level analysis. The '2040 Volume E+C' is an estimate of the volume in 2040 with only existing plus committed projects assumed to be in place, where committed is defined as projects programmed for construction in the 2012 - 2020 Transportation Improvement Program (TIP). The '2040 Volume with CTP' is an estimate of the volume in 2040 with all proposed CTP improvements assumed to be in place. The '2040 Volume with CTP' is shown in bold if it exceeds the proposed capacity, indicating an unmet need. For additional information about the assumptions and techniques used to develop the AADT volume estimates, refer to Chapter 1.
- ❖ **Proposed Cross-section:** The CTP recommended cross-sections are listed by code; for depiction of the cross-section, refer to Appendix D. An entry of 'ADQ' indicates the existing facility is adequate and there are no improvements recommended for the given mode as part of the CTP.

- ❖ **CTP Classification:** The CTP classification is listed, as shown on the adopted CTP Maps (see Figure 1). Abbreviations are F= freeway, E= expressway, B= boulevard, Maj= other major thoroughfare, Min= minor thoroughfare.
- ❖ **Tier:** Tiers are defined as part of the North Carolina Multimodal Investment Network (NCMIN). Abbreviations are Sta= statewide tier, Reg= regional tier, Sub= subregional tier.
- ❖ **Proposals for Other Modes:** If there is an improvement recommended for another mode of transportation that relates to the given recommendation, it is indicated by an alphabetic code (H= highway, T= public transportation, R= rail, B= bicycle, P= pedestrian, and M= multi-use path).

DRAFT

CTP INVENTORY AND RECOMMENDATIONS

HIGHWAY																				
Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
						Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
		From	To																	
RAND0068-H	I-73/I-74 (US-220 Bypass)	Asheboro Northern City Limits-	Pineview Rd (SR 1712)	Randolph Co.	1.23	48	4D	12	130	65	55900	34000	60000	60800	84600	6A	300	F	Sta	
RAND0068-H	I-73/I-74 (US-220 Bypass)	Pineview Rd (SR 1712)	Sparo Rd (SR 1504)	Randolph Co.	1.4	48	4D	12	130	65	55900	33000	60000	60800	84600	6A	300	F	Sta	
RAND0068-H	I-73/I-74 (US-220 Bypass)	Sparo Rd (SR 1504)	Vision Dr (SR 2269)	Randolph Co.	1.4	48	4D	12	130	65	55900	36000	60000	60800	84600	6A	300	F	Sta	
RAND0068-H	I-73/I-74 (US-220 Bypass)	Vision Dr (SR 2269)	West Presnell St (SR 1462)	Asheboro	0.9	48	4D	12	130	65	55900	36000	62000	62500	84600	6A	300	F	Sta	
RAND0069-H	I-73/I-74 (US-220 Bypass)	West Presnell St (SR 1462)	NC42 (W Salisbury St)	Asheboro	0.8	48	4D	12	130	65	55900	36000	64000	66000	84600	6A	300	F	Sta	
RAND0069-H	I-73/I-74 (US-220 Bypass)	NC42 (W Salisbury St)	Sunset Ave (SR 1442)	Asheboro	0.1	48	4D	12	130	55	55900	36000	62000	63000	84600	6A	300	F	Sta	
RAND0069-H	I-73/I-74 (US-220 Bypass)	Sunset Ave (SR 1442)	Old Farmer Rd (SR 3255)	Asheboro	0.4	48	4D	12	130	55	55900	34000	62000	63000	84600	6A	300	F	Sta	
RAND0069-H	I-73/I-74 (US-220 Bypass)	Old Farmer Rd (SR 3255)	Albemarle Rd (SR 1713)	Asheboro	1	48	4D	12	130	55	55900	34000	62000	63000	84600	6A	300	F	Sta	
RAND0069-H	I-73/I-74 (US-220 Bypass)	Albemarle Rd (SR 1713)	US 64 (W Dixie Dr)	Asheboro	0.1	48	4D	12	130	55	55900	34000	62000	66000	84600	6A	300	F	Sta	
	I-73/I-74 (US-220 Bypass)	US 64 (W Dixie Dr)	McDowell Rd (SR 1150)	Asheboro	1.1	48	4D	12	130	55	55900	28000	40000	46000	55900	ADQ	ADQ	F	Sta	
	I-73/I-74 (US-220 Bypass)	McDowell Rd (SR 1150)	Proposed Ashebor Southern Bypass	Asheboro	1.5	48	4D	12	130	55	55900	22000	35000	36000	55900	ADQ	ADQ	F	Sta	T
	I-73/I-74 (US-220 Bypass)	Proposed Asheboro Southern Bypass	Pisgah Covered Bridge Rd (SR 1114)	Randolph Co.	1.3	48	4D	12	130	55	55900	20000	36000	38000	55900	ADQ	ADQ	F	Sta	
	I-73/I-74 (US-220 Bypass)	Pisgah Covered Bridge Rd (SR 1114)	Dawson Miller Rd (SR 1138)	Randolph Co.	0.8	48	4D	12	130	55	55900	20000	36000	38000	55900	ADQ	ADQ	F	Sta	
	I-73/I-74 (US-220 Bypass)	Dawson Miller Rd (SR 1138)	Southern PAB	Randolph Co.	0.8	48	4D	12	130	55	55900	13000	24000	30000	55900	ADQ	ADQ	F	Sta	
RAND0003-H	US 64 /NC 49	Pleasant Cross Rd (SR 2224)	Loflin Pond Rd (SR 2221)	Randolph Co.	0.7	60	5	12	130	55	31800	17000	26000	26200	55900	4A	150	F	Sta	

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
RAND0003-H	US 64 /NC 49	Loffin Pond Rd (SR 2221)	Proposed Asheboro Southern Bypass	Randolph Co.	0.8	60	5	12	130	55	31800	19000	28000	28500	55900	4A	150	F	Sta	
RAND0001-H	US 64 /NC 49	Proposed Asheboro Southern Bypass	E Presnell St (SR 1462)	Randolph Co.	0.9	48	4D	12	130	45	40500	20000	21000	21500	36600	4D	150	B	Sta	
RAND0001-H	US 64 /NC 49	E Presnell St (SR 1462)	Luck Rd (SR 2604)	Randolph Co.	0.5	60	5	12	130	45	27600	20000	20000	20500	36600	4D	150	B	Sta	
RAND0001-H	US 64 /NC 49	Luck Rd (SR 2604)	E Salisbury St (SR 2237)	Asheboro	0.6	60	5	12	130	45	27600	20000	20000	20500	36600	4D	150	B	Sta	
RAND0001-H	US 64 /NC 49 (E Dixie Dr)	E Salisbury St (SR 2237)	NC 42	Asheboro	0.6	60	5	12	130	45	27600	21000	20000	21000	36600	4D	150	B	Sta	P
RAND0001-H	US 64 /NC 49 (E Dixie Dr)	NC 42	Dublin Rd (SR 2197)	Asheboro	0.6	60	5	12	130	45	27600	30000	30500	32900	36600	4D	150	B	Sta	P,T
RAND0001-H	US 64 /NC 49 (E Dixie Dr)	Dublin Rd (SR 2197)	Cliff Rd (SR 2203)	Asheboro	0.6	60	5	12	130	45	27600	32000	35000	36400	36600	4D	150	B	Sta	P,T
RAND0001-H	US 64 /NC 49 (E Dixie Dr)	Cliff Rd (SR 2203)	Cox St (SR 2327)	Asheboro	0.3	60	5	12	130	45	27600	32000	35000	36400	36600	4D	150	B	Sta	P,T
RAND0001-H	US 64 /NC 49 (E Dixie Dr)	Cox St (SR 2327)	US 220 Bus (S Fayetteville St)	Asheboro	0.3	60	5	12	130	45	27600	31000	34000	34200	36600	4D	150	B	Sta	P,T
RAND0001-H	US 64 /NC 49 (W Dixie Dr)	US 220 Bus (S Fayetteville St)	S Park St (SR 1451)	Asheboro	0.2	60	5	12	130	45	27600	28000	31800	32800	36600	4D	150	B	Sta	P,T
RAND0001-H	US 64 /NC 49 (W Dixie Dr)	S Park St (SR 1451)	I-73/I-74 (US-220 Bypass)	Asheboro	0.6	60	5	12	130	45	27600	28000	32000	34000	36600	4D	150	B	Sta	P
R-2220	US 64 /NC 49 (W Dixie Dr)	I-73/I-74 (US-220 Bypass)	Albamrale Rd (SR 1713)	Asheboro	0.3	48	4D	12	150	45	36600	22000	21000	21500	36600	4D	150	B	Sta	
R-2220	US 64	Albamrale Rs (SR 1713)	Old Farmer Rd (SR 3255)	Randolph Co.	0.9	24	3	12	150	55	15900	12000	14800	15500	36600	4D	150	B	Sta	
R-2220	US 64	Old Farmer Rd (SR 3255)	Proposed Asheboro Southern Bypass	Randolph Co.	1.4	24	2	12	150	55	14600	11000	14000	14600	36600	4D	150	B	Sta	
R-2220	US 64	Proposed Asheboro Southern Bypass	Stutts Rd (SR 1326)	Randolph Co.	0.3	24	2	12	150	55	14600	11000	13000	14000	53300	4B	150	E	Sta	
R-2220	US 64	Stutts Rd (SR 1326)	Western PAB	Randolph Co.	0.9	24	2	12	150	55	14600	11000	14500	15200	53300	4B	150	E	Sta	

HIGHWAY																				
Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
						Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
		From	To																	
R-2536	Proposed Asheboro Southern Bypass	US 64 East of Asheboro	Luck Rd (SR 2604)	Randolph Co.	1.3	-	-	-	-	-	-	10000	10000	55900	4A	300	F	Sta		
R-2536	Proposed Asheboro Southern Bypass	Luck Rd (SR 2406)	NC 42	Asheboro	1.9	-	-	-	-	-	-	10000	10000	55900	4A	300	F	Sta		
R-2536	Proposed Asheboro Southern Bypass	NC 42	Pine Hill Rd (SR 2824)	Asheboro	1	-	-	-	-	-	-	11500	11500	55900	4A	300	F	Sta		
R-2536	Proposed Asheboro Southern Bypass	Pine Hill Rd (SR 2824)	Old Cox Rd (SR 2834)	Asheboro	0.86	-	-	-	-	-	-	11500	11500	55900	4A	300	F	Sta		
R-2536	Proposed Asheboro Southern Bypass	Old Cox Rd (SR 2834)	NC 159	Asheboro	0.3	-	-	-	-	-	-	11500	11500	55900	4A	300	F	Sta		
R-2536	Proposed Asheboro Southern Bypass	NC 159	Proposed Zoo Connector	Asheboro	0.8	-	-	-	-	-	-	11000	11000	55900	4A	300	F	Sta		
R-2536	Proposed Asheboro Southern Bypass	Proposed Zoo Connector	US 220 Bus (Fayetteville St)	Asheboro	1.6	-	-	-	-	-	-	13200	13200	55900	4A	300	F	Sta	T	
R-2536	Proposed Asheboro Southern Bypass	US 220 Bus (Fayetteville St)	I-73/I-74 (US-220 Bypass)	Asheboro	0.5	-	-	-	-	-	-	13200	13200	55900	4A	300	F	Sta	T	
R-2536	Proposed Asheboro Southern Bypass	I-73/I-74 (US-220 Bypass)	Mack Rd (SR 1144)	Asheboro	1.2	-	-	-	-	-	-	11000	11000	55900	4A	300	F	Sta		

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
R-2536	Proposed Asheboro Southern Bypass	Mack Rd (SR 1144)	NC 49	Randolph Co.	1.5	-	-	-	-	-	-	-	11000	11000	55900	4A	300	F	Sta	
R-2536	Proposed Asheboro Southern Bypass	NC 49	Old NC Hwy 49 (SR 1193)	Randolph Co.	0.5	-	-	-	-	-	-	-	5200	5200	55900	4A	300	F	Sta	
R-2536	Proposed Asheboro Southern Bypass	Old Hwy 49 (SR 1193)	US 64 West of Asheboro	Randolph Co.	1.3	-	-	-	-	-	-	-	5200	5200	55900	4A	300	F	Sta	
R-2536	Zoo Connector	Proposed Asheboro Southern Bypass	NC 159	Randolph Co.	-	-	-	-	-	-	-	-	3300	3300	13800	3A	80	Maj	—	T
U-3600	US 220 Bus (Fayetteville St)	Northern PAB	Pineview Rd (SR 1712)	Asheboro	1.4	26	2	12	100	45	12700	11000	16600	16600	26000	4D	100	Maj	Reg	T
U-3600	US 220 Bus (Fayetteville St)	Pineview Rd (SR 1712)	Hub Morris Rd (SR 2149)	Asheboro	0.2	36	3	12	100	45	13800	12000	17600	17600	26000	4D	100	Maj	Reg	T
U-3600	US 220 Bus (Fayetteville St)	Hub Morris Rd (SR 2149)	W Central Ave (SR 1504)	Asheboro	1.05	24	2	12	100	45	12700	12000	17600	17600	26000	4D	100	Maj	Reg	T
U-3600	US 220 Bus (Fayetteville St)	W Central Ave (SR 1504)	W Balfour Ave (SR 1502)	Asheboro	0.6	24	2	12	100	45	12700	11000	14700	14700	26000	4D	100	Maj	Reg	T
U-3600	US 220 Bus (Fayetteville St)	W Balfour Ave (SR 1502)	Vision Dr (SR 2269)	Asheboro	0.6	36	2-3	12	100	35	12900	12000	16600	16600	26000	4D	100	Maj	Reg	T
	US 220 Bus (Fayetteville St)	Vision Dr (SR 2269)	E Allred St (SR 2182)	Asheboro	0.5	64	5	12	100	35	26000	14000	18000	18000	26000	ADQ	ADQ	Maj	Reg	T
	US 220 Bus (Fayetteville St)	E Allred St (SR 2182)	E Pritchard St	Asheboro	1.0	64	5	12	100	35	26000	14000	18000	18500	26000	ADQ	ADQ	Maj	Reg	T
	US 220 Bus (Fayetteville St)	E Prichard St	E Presnell St (SR 1462)	Asheboro	0.2	48	4	12	100	35	23500	16000	18500	18800	23500	ADQ	ADQ	Maj	Reg	P,T
RAND0049-H	US 220 Bus (Fayetteville St)	E Presnell St (SR 1462)	NC 42	Asheboro	0.7	48	4	12	100	35	23500	26000	27000	28000	23500	TDM	ADQ	Maj	Reg	P,T

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	US 220 Bus (Fayetteville St)	NC 42	Sunset Ave (SR 1442)	Asheboro	0.1	48	4	12	100	20	23500	18000	18600	18600	23500	ADQ	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	Sunset Ave (SR 1442)	Academy St	Asheboro	0.2	48	4	12	100	20	23500	18000	18600	18600	23500	ADQ	ADQ	Maj	Reg	P,T
RAND0049-H	US 220 Bus (Fayetteville St)	Academy St	Kivett St	Asheboro	0.3	36	3	12	100	20	12900	15000	16000	17300	12900	TDM	ADQ	Maj	Reg	P,T
RAND0049-H	US 220 Bus (Fayetteville St)	Kivett St	Birkhead Dr	Asheboro	0.7	36	3	12	100	20	12900	16000	17000	17700	12901	TDM	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	Birkhead Dr	W Walker Ave	Asheboro		48	4	12	100	20	23500	16000	17000	17700	23500	ADQ	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	W Walker Ave	US 64 (W Dixie Dr)	Asheboro	0.2	48	4-4D	12	100	20	23500	11000	16000	17000	23500	ADQ	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	US 64 (W Dixie Dr)	Country Club Dr (SR 1154)	Asheboro	0.1	48	4D	12	100	35	23500	10000	12000	12000	23500	ADQ	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	Country Club Dr (SR 1154)	Ridge Rd (SR 2915)	Asheboro	0.2	36-48	3-4D	12	100	35	12900	10000	11000	11000	23500	ADQ	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	Ridge Rd (SR 2915)	Eldorado Rd (SR 2919)	Asheboro	0.7	36	3	12	100	35	12900	9000	10000	10000	12900	ADQ	ADQ	Maj	Reg	P,T
	US 220 Bus (Fayetteville St)	Eldorado Rd (SR 2919)	McDowell Rd (SR 1150)	Asheboro	0.3	36	3	12	100	45	13800	9000	10000	10000	13800	ADQ	ADQ	Maj	Reg	T
	US 220 Bus (Fayetteville St)	McDowell Rd (SR 1150)	Southmont Dr (SR 1145)	Randolph Co.	0.2	36	3	12	100	45	13800	7000	9000	9000	13800	ADQ	ADQ	Maj	Reg	
	US 220 Bus (Fayetteville St)	Southmont Dr (SR 1145)	Crestview Church Rd (SR 2820)	Randolph Co.	0.8	24	2	12	100	45	13800	7000	9000	9000	13800	ADQ	ADQ	Maj	Reg	
	US 220 Bus (Fayetteville St)	Crestview Church Rd (SR 2820)	Proposed Asheboro Southern Bypass	Randolph Co.	0.4	24	2	12	100	45	13800	7000	9000	9000	13800	ADQ	ADQ	Maj	Reg	
	US 220 Bus (Fayetteville St)	Proposed Asheboro Southern Bypass	Pisgah Covered Bridge Rd (SR 1114)	Randolph Co.	0.9	24	2	12	100	55	14600	7000	10000	10000	14600	ADQ	ADQ	Maj	Reg	
	US 220 Bus (Fayetteville St)	Pisgah Covered Bridge Rd (SR 1114)	Dawson Miller Rd (SR 1138)	Randolph Co.	1	24	2	12	100	55	14600	7000	9000	9000	14600	ADQ	ADQ	Maj	Reg	
	US 220 Bus ALT	Dawson Miller Rd (SR 1138)	Southern PAB	Randolph Co.	1	24	2	12	100	55	14600	6500	7500	7500	14600	ADQ	ADQ	Maj	Reg	

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Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
						Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
		From	To																	
RAND0072-H	NC 42	Old Humble Mill Road (SR 2830)	Iron Mountain Rd (SR 2605)	Randolph Co.	0.2	20	2	10	150	55	13600	5000	5600	5600	14600	2A	150	Maj	Reg	
RAND0072-H	NC 42	Iron Mountain Rd (SR 2605)	Proposed Asheboro Southern Bypass	Randolph Co.	0.8	20	2	10	150	55	13600	7600	9300	9300	14600	2A	150	Maj	Reg	
RAND0072-H	NC 42	Proposed Asheboro Southern Bypass	Browers Chapel Rd (SR 2826)	Randolph Co.	1.2	20	2	10	150	55	13600	8000	8500	8500	14600	2A	150	Maj	Reg	
RAND0070-H	NC 42	Browers Chapel Rd (SR 2826)	US 64 (Dixie Dr)	Randolph Co.	0.9	20	2	10	150	35-55	13600	13000	13600	13600	26000	4A	150	Maj	Reg	
RAND0070-H	NC 42	US 64 (Dixie Dr)	Dublin Rd (SR 2197)	Asheboro	0.5	24	2	12	150	35	11600	13000	14200	14200	11600	4A	150	Maj	Reg	P,T
RAND0070-H	NC 42	Dublin Rd (SR 2197)	Salisbury St (SR 2237)	Asheboro	0.3	24	2	12	60	35	11600	13000	14200	14200	11600	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (E Salisbury St)	Salisbury St (SR 2237)	Martin Luther King Jr Dr (SR 2189)	Asheboro	0.5	24	2	12	60	35	11600	13000	14100	14100	11600	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (E Salisbury St)	Martin Luther King Jr Dr (SR 2189)	Elm St	Asheboro	0.1	36	3	12	60	35	12900	13000	15500	15500	12900	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (E Salisbury St)	Elm St	Cox St (SR 2327)	Asheboro	0.3	33	3	11	60	35	12900	13000	15500	15500	12900	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (E Salisbury St)	Cox St (SR 2327)	US 220 Bus (Fayetteville St)	Asheboro	0.1	33	3	11	60	35	12900	13500	16300	16300	12900	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (W Salisbury St)	US 220 Bus (Fayetteville St)	Church St (SR1707)	Asheboro	0.2	33	3	11	60	35	12900	12000	12300	12300	12900	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (W Salisbury St)	Church St (SR1707)	S Park St (SR 1451)	Asheboro	0.2	24	2	12	60	35	11600	11000	11500	11500	11600	TDM	ADQ	Maj	Reg	P,T
RAND0050-H	NC 42 (W Salisbury St)	S Park St (SR 1451)	N Cherry St	Asheboro	0.3	24	2	12	60	35	11600	8800	10000	10000	11600	TDM	ADQ	Maj	Reg	P
RAND0050-H	NC 42 (W Salisbury St)	N Cherry St	I-73/I-74 (US-220 Bypass)	Asheboro	0.2	24	2	12	60	35	11600	8800	10000	10000	11600	TDM	ADQ	Maj	Reg	P
RAND0050-H	NC 42 (W Salisbury St)	I-73/I-74 (US-220 Bypass)	Sunset Ave (SR 1442)	Asheboro	0.2	48	4D	12		35	36600	8800	10000	10000	3600	ADQ	ADQ	Maj	Reg	P

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Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
U-5305	NC 49	US 64	Old NC Hwy 49 (SR 1193)	Asheboro	0.8	24	2	12	150	35	12900	12000	12800	12800	13800	3A	150	Maj	Reg	T
	NC 49	Old NC Hwy 49 (SR 1193)	Proposed Asheboro Southern Bypass	Randolph Co.	0.9	24	2	12	190	55	14600	8200	9100	9100	14600	ADQ	ADQ	Maj	Reg	
R-2535B	NC 49	Proposed Asheboro Southern Bypass	Tot Hill Farms Rd (SR 1163)	Randolph Co.	1.5	24	2	12	190	55	14600	6700	10200	10200	14600	4A	190	E	Sta	
R-2535B	NC 49	Tot Hill Farms Rd (SR 1163)	Western PAB	Randolph Co.	2.5	24	2	12	190	55	14600	5800	9000	9000	14600	4A	190	E	Sta	
	NC 159	US 64/ NC 49 (E Dixie Dr)	Ridge Rd (SR 2915)	Asheboro	0.3	24	2	12	60	35	11600	9000	8800	8800	11600	ADQ	ADQ	Maj	Sub	
	NC 159	Ridge Rd (SR 2915)	Newbern Ave (SR 2919)	Asheboro	0.7	24	2	12	60	35	11600	9000	8800	8800	11600	ADQ	ADQ	Maj	Sub	
	NC 159	Newbern Ave (SR 2919)	Crestview Church Rd (SR 2820)	Randolph Co.	0.7	24	2	12	60	35	11600	8500	8600	8600	11600	ADQ	ADQ	Maj	Sub	
	NC 159	Crestview Church Rd (SR 2820)	Old Cox Rd (SR 2834)	Randolph Co.	0.4	24	2	12	60	35	11600	7000	9100	9100	11600	ADQ	ADQ	Maj	Sub	
	NC 159	Old Cox Rd (SR 2834)	Proposed Asheboro Southern Bypass	Randolph Co.	0.4	24	2	12	60	55	14600	6100	6200	6200	14600	ADQ	ADQ	Maj	Sub	
	NC 159	Proposed Asheboro Southern Bypass	Zoo Parkway (NC 159 - Spur)	Randolph Co.	1.8	24	2	12	60	55	14600	2500	4200	4200	14600	ADQ	ADQ	Maj	Sub	
	NC 159	Zoo Parkway (NC 159 - Spur)	Southern PAB	Randolph Co.	2.2	24	2	12	60	55	14600	2000	4000	4000	14600	ADQ	ADQ	Maj	Sub	
	NC 159 Spur	NC 159	Old Cox Rd (SR 2834)	Randolph Co.	0.7	24	2	12	500	35	11600	1200	2800	2800	11600	ADQ	ADQ	Maj	Sub	T
	NC 159 Spur	Old Cox Rd (SR 2834)	NC Zoo	Randolph Co.	0.1	24	2	12	500	35	11600	1200	2800	2800	11600	ADQ	ADQ	Maj	Sub	T
	Albemarle Rd (SR 1713)	US 64 / NC 49 (E Dixie Dr)	73/1-74 (US-220 Bypass)	Asheboro	0.3	24	2	12	60	35	15000	9000	11500	11500	15000	ADQ	ADQ	Min	Sub	T

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Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	Albemarle Rd (SR 1713)	73/1-74 (US-220 Bypass)	Uwharrie St (SR 1443)	Asheboro	0.2	24	2	12	60	35	10200	6600	7500	7500	10200	ADQ	ADQ	Min	Sub	T
	Albemarle Rd (SR 1713)	Uwharrie St (SR 1443)	W Walker Ave (SR 1453)	Asheboro	0.4	27	2	12	60	35	10200	6600	7500	7500	10200	ADQ	ADQ	Min	Sub	T
	Albemarle Rd (SR 1713)	W Walker Ave (SR 1453)	S Park St (SR1451)	Asheboro	0.2	23	2	11	60	35	10200	6600	7500	7500	10200	ADQ	ADQ	Min	Sub	T
	Albemarle Rd (SR 1713)	S Park St (SR1451)	S Church St (SR 1707)	Asheboro	0.1	23	2	11	60	35	10200	6600	7500	7500	10200	ADQ	ADQ	Min	Sub	T
	Allred Rd (SR 2182)	Medowbrook Rd (SR 2184)	Gold Hill Rd (SR 2183)	Asheboro-Randolph Co.	1.5	20	2	10	60	35	9500	800	1200	1200	9500	ADQ	ADQ	Min	Sub	
	Back Creek Ch Rd (SR 1327)	US 64	Stutts Rd (SR 1326)	Randolph Co.	0.7	20	2	10	60	55	14100	3800	4700	4700	14100	ADQ	ADQ	Min	Sub	
	Bell Simmons Rd (SR 1146)	Southmont Dr (SR 1145)	US 220 Bus (Fayetteville St)	Randolph Co.	1	20	2	10	-	55	13600	1300	2000	2000	14600	2A	60	Min	Sub	
RAND0016-H	Bowers Chapel Rd (SR 2826)	US 64 / NC 49 (E Dixie Dr)	Proposed Crestview Church Rd Ext.	Randolph Co.	1.6	20	2	10	60	55	13600	1300	2200	2200	14600	2A	60	Min	Sub	
RAND0016-H	Bowers Chapel Rd (SR 2826)	Proposed Crestview Church Rd Ext.	Pine Hill Rd (SR 2824)	Randolph Co.	0.3	20	2	10	60	55	13600	800	1500	1500	14600	2A	60	Min	Sub	
RAND0016-H	Bowers Chapel Rd (SR 2826)	Pine Hill Rd (SR 2824)	NC 42	Randolph Co.	1.1	18	2	9	60	55	13100	600	1500	1500	14600	2A	60	Min	Sub	
	Cable Creek Rd (SR 1320)	Old NC 49 (SR 1193)	Stutts Rd (SR 1326)	Randolph Co.	0.7	20	2	10	-	55	14100	4000	4500	4500	14100	ADQ	ADQ	Min	Sub	
	Central Farm Rd (SR 2263)	Old Liberty Rd (SR 2261) (East)	Old Liberty Rd (SR 2261) (West)	Asheboro	0.8	22	2	11	60	35	9900	1800	2500	2500	10200	ADQ	ADQ	Min	Sub	
	Charles Ave (SR 2812)	US 220 Bus (Fayetteville St)	Eldorado Rd (SR 2919)	Randolph Co.	0.1	18	2	9	55	55	13100	1100	2000	2000	13100	ADQ	ADQ	Min	Sub	

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		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	Cliff Rd	S Elm St	E Kivett St	Asheboro	0.4	24	2	12	60	35	10200	2700	4500	4500	10200	ADQ	ADQ	Min	Sub	P
	Cliff Rd	E Kivett St	US 64 /NC 49 (E Dixie Dr)	Asheboro	0.6	38	2	12	60	35	10200	2700	4500	4500	10200	ADQ	ADQ	Min	Sub	P
	Coleridge Rd (SR 2194)	Martin Luther King Jr Dr. (SR 2189)	NC 42 (E Salisbury St)	Asheboro	0.2	36	3	12	60	35	9200	2100	4000	4000	9200	ADQ	ADQ	Min	Sub	
	Country Club Rd (SR)	S Park St (SR 1451)	US 220 Bus (Fayetteville St)	Asheboro	0.2	20	2	10	-	35	9500	800	1500	1500	9500	ADQ	ADQ	Min	Sub	
RAND0019-H	Crestview Church Rd (SR 2820)	US 220 Bus (Fayetteville St)		Randolph Co.	2	20	2	10	60	55	13600	1600	2800	2800	14600	2A	60	Min	Sub	
	Dawson Miller Rd (SR 1138)	Pisgah Covered Bridge Rd (SR 1114)	I-73/I-74 (US-220 Bypass)	Randolph Co.	1.3	20	2	10	-	55	14100	1500	2500	2500	14100	ADQ	ADQ	Min	Sub	
RAND0020-H	Danny Bell Rd (SR 1162)	Mack Rd (SR 1144)	Southern PAB	Randolph Co.	2.5	20	2	10	60	55	13600	1600	2500	2500	14600	2A	60	Min	Sub	
RAND0051-H	Dixon Ave (SR 1443)	S Cherry St (SR 1443)	Uwharrie St (SR 1443)	Asheboro	0.1	22	2	11	-	35	9900	1500	2000	2000	10200	2A	60	Min	Sub	
RAND0052-H	Drapper St(SR 2159)	Central Farm Rd (SR 2263)	Gold Hill Rd (SR 2183)	Asheboro	0.4	20	2	10	60	35	9500	1400	2500	2500	10200	2A	60	Min	Sub	
RAND0053-H	Dublin Rd (SR 2197)	US 64 /NC 49 (E Dixie Dr)	NC 42 (E Salisbury St)	Asheboro	0.6	18	2	9	60	35	9200	2900	4800	4800	10200	2E	60	Min	Sub	P
	E Allred Rd (SR 2182)	US 220 Bus (Fayetteville St)	Medowbrook Rd (SR 2184)	Asheboro	0.4	24	2	12	60	35	10200	2600	3800	3800	10200	ADQ	ADQ	Min	Sub	
	E Balfour Ave (SR 1502)	US 220 Bus (Fayetteville St)	Old Liberty Rd (SR 2261)	Asheboro	0.4	24	2	12	60	35	10200	2500	3900	3900	10200	ADQ	ADQ	Min	Sub	

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		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section				ROW (ft)
	E Kivett St	Uwharrie St (SR 1443)	S Park St (SR 1451)	Asheboro	0.3	24	2	12	60	35	10200	3500	5000	5000	10200	ADQ	ADQ	Min	Sub	P
	E Kivett St	S Park St (SR 1451)	Church St (SR 1707)	Asheboro	0.2	24	2	12	60	35	10200	3500	5000	5000	10200	ADQ	ADQ	Min	Sub	P
	E Kivett St	Church St (SR 1707)	US 220 Bus (Fayetteville St)	Asheboro	0.2	24	2	12	60	35	10200	3500	5000	5000	10200	ADQ	ADQ	Min	Sub	P
	E Presnell St (SR 1462)	US 220 Bus (Fayetteville St)	N Cox St	Asheboro	0.1	24	2	12	60	35	10200	6000	7600	7600	10200	ADQ	ADQ	Min	Sub	
	E Presnell St (SR 1462)	N Cox St	Medowbrook Rd (SR 2184)	Asheboro	0.3	26	2	12	50	35	10200	5000	6200	6200	10200	ADQ	ADQ	Min	Sub	B,T
	E Presnell St (SR 1462)	Medowbrook Rd (SR 2184)	Gold Hill Rd (SR 2183)	Asheboro	0.8	26	2	12	60	35	10200	4500	5700	5700	10200	ADQ	ADQ	Min	Sub	B
	E Presnell St (SR 1462)	Gold Hill Rd (SR 2183)	Old Cedar Falls Rd (SR 2216)	Randolph Co.	0.1	24	2	12	60	45	12200	3500	4500	4500	12200	ADQ	ADQ	Min	Sub	B
	E Presnell St (SR 1462)	Old Cedar Falls Rd (SR 2216)	Henry Country Rd (SR 2215)	Randolph Co.	1.5	24	2	12	60	45	12200	3500	4500	4500	12200	ADQ	ADQ	Min	Sub	
	E Presnell St (SR 1462)	Henry Country Rd (SR 2215)	US 64/ NC 49	Randolph Co.	1.5	24	2	12	60	45	12200	3500	4500	4500	12200	ADQ	ADQ	Min	Sub	
RAND0054-H	E Pritchard St	US 220 Bus (Fayetteville St)	Medowbrook Rd (SR 2184)	Asheboro	0.4	20	2	10	-	35	9500	1100	1800	1800	10200	2A	60	Min	Sub	
	Eldorado Rd (SR 2919)	Charles Ave (SR 2812)	N Hampton Dr	Randolph Co.	1	18	2	9	60	55	13100	900	2000	2000	13100	ADQ	ADQ	Min	Sub	
RAND0055-H	Giles Chapel Rd (SR 2218)	Gold Hill Rd (SR 2183)	Henry Country Rd (SR 2215)	Randolph Co.	1	18	2	9	-	55	13100	2000	3000	3000	14600	2A	60	Min	Sub	
RAND0056-H	Gold Hill Rd (SR 2183)	Old Liberty Rd (SR 2261)	Giles Chapel Rd (SR 2218)	Asheboro	0.5	20	2	10	-	35	9500	3000	4800	4800	10200	2E	60	Min	Sub	B
RAND0056-H	Gold Hill Rd (SR 2183)	Giles Chapel Rd (SR 2218)	Draper St (SR 2159)	Asheboro	0.2	20	2	10	-	35	9500	3000	4800	4800	10200	2E	60	Min	Sub	B
RAND0056-H	Gold Hill Rd (SR 2183)	Draper St (SR 2159)	Allred St (SR 2182)	Asheboro	0.7	20	2	10	-	35	9500	3000	4500	4500	10200	2E	60	Min	Sub	B
RAND0056-H	Gold Hill Rd (SR 2183)	Allred St (SR 2182)	E Presnell St (SR 1462)	Randolph Co.	1.6	18	2	9	-	55	13100	3000	4500	4500	14600	2E	60	Min	Sub	B

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		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
RAND0071-H	Henly Country Rd (SR 2215)	Giles Chapel Rd (SR 2218)	Old Cedar Falls Rd (SR 2216)	Randolph Co.	0.9	24	2	12	-	55	14600	4000	5900	5900	14600	2A	60	Min	Sub	
RAND0025-H	Henly Country Rd (SR 2215)	Old Cedar Falls Rd (SR 2216)	US 64 /NC 49 (E Dixie Dr)	Randolph Co.	1.4	20	2	10	50	55	13600	1100	2200	2200	14600	2A	60	Min	Sub	
RAND0057-H	Hopewell Friends Rd(SR 1142)	Tot Hill Farms Rd (SR 1163)	Denny Bells Rd (SR 1162)	Randolph Co.	0.9	21	2	10	-	55	13600	3600	4500	4500	14600	2A	60	Min	Sub	B
RAND0057-H	Hopewell Friends Rd(SR 1142)	Denny Bells Rd (SR 1162)	Mack Rd (SR 1144)	Randolph Co.	1.4	21	2	10	-	55	13600	3600	4500	4500	14600	2A	60	Min	Sub	B
RAND0057-H	Hopewell Friends Rd(SR 1142)	Mack Rd (SR 1144)	Pisgah Covered Bridge Rd (SR 1114)	Randolph Co.	0.9	18	2	10	-	55	13100	3000	4100	4100	14600	2A	60	Min	Sub	
RAND0072-H	Hub Morris Rd (SR 2149)	US 220 Bus (Fayetteville St)	Old Liberty Rd (SR 2261)	Asheboro/Randolph Co.	1.73	18	2	9	60	35-55	9200	4500	6500	6500	9200	2A	60	Min	Sub	
	Iron Mountain Rd (SR 2605)	NC 42	Luck Rd (SR 2604)	Randolph Co.	1.3	22	2	11	-	55	14600	1000	1800	1800	14600	ADQ	ADQ	Min	Sub	
	Iron Mountain Rd (SR 2605)	Luck Rd (SR 2604)	US 64	Randolph Co.	2.5	22	2	11	-	55	14600	1000	1800	1800	14600	ADQ	ADQ	Min	Sub	
RAND0073-H	Lassiter Mill Road (SR 1107)	NC 49	Old NC 49	Randolph Co.	1.2	24	2	10	-	55	13600	2000	2800	2800	13600	2E	60	Min		B
RAND0033-H	Luck Rd (SR 2604)	US 64 /NC 49 (E Dixie Dr)	Proposed Asheboro Southern Bypass	Randolph Co.	1	18	2	9	60	55	13100	1400	3000	3000	14600	2A	60	Min	Sub	
RAND0033-H	Luck Rd (SR 2604)	Proposed Asheboro Southern Bypass	Eastern PAB	Randolph Co.	1.7	20	2	10	60	55	13600	500	900	900	13600	2A	60	Min	Sub	
	Mack Rd (SR 1144)	NC 49	McDowell Rd (SR 1150)	Randolph Co.	0.9	24	2	12	-	55	14600	5200	6500	6500	14600	ADQ	ADQ	Min	Sub	

HIGHWAY																				
Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
						Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
		From	To																	
	Mack Rd (SR 1144)	McDowell Rd (SR 1150)	Proposed Asheboro Southern Bypass	Randolph Co.	1.5	24	2	12	-	55	14600	4000	6100	6100	14600	ADQ	ADQ	Min	Sub	
	Mack Rd (SR 1144)	Proposed Asheboro Southern Bypass	Danny Ball Rd (SR 1162)	Randolph Co.	0.2	24	2	12	-	55	14600	3500	4500	4500	14600	ADQ	ADQ	Min	Sub	
	Mack Rd (SR 1144)	Danny Ball Rd (SR 1162)	Southmont Dr (SR 1145)	Randolph Co.	0.2	24	2	12	-	55	14600	3300	4500	4500	14600	ADQ	ADQ	Min	Sub	
	Mack Rd (SR 1144)	Southmont Dr (SR 1145)	Southern PAB	Randolph Co.	2.1	24	2	12	-	55	14600	3300	4500	4500	14600	ADQ	ADQ	Min	Sub	
RAND0058-H	Martin Luther King Jr Dr. (SR 2189)	NC 42 (E Salisbury St)	Old Cedar Falls Rd (SR 2216)	Asheboro	0.4	18	2	9	60	35	9200	2000	4500	4500	10200	2F	60	Min	Sub	P
RAND0058-H	Martin Luther King Jr Dr. (SR 2189)	Old Cedar Falls Rd (SR 2216)	Coleridge Rd (SR 2194)	Asheboro	0.1	18	2	9	60	35	9200	2000	4500	4500	10200	2F	60	Min	Sub	P
RAND0058-H	Martin Luther King Jr Dr. (SR 2189)	Coleridge Rd (SR 2194)	NC 42 (E Salisbury St)	Asheboro	0.5	18	2	9	60	35	9200	1000	1900	1900	10200	2A	60	Min	Sub	
RAND0059-H	McDowell Rd (SR 1150)	Mack Rd (SR 1144)	73/I-74 (US-220 Bypass)	Randolph Co.	0.5	20	2	10	-	55	13600	5600	6800	6800	14600	2A	60	Min	Sub	
RAND0059-H	McDowell Rd (SR 1150)	73/I-74 (US-220 Bypass)	US 220 Bus (Fayetteville St)	Randolph Co.	0.6	18	2	9	-	55	13100	6500	7800	7800	14600	2A	60	Min	Sub	
RAND0060-H	Medowbrook Rd (SR 2184)	E Presnell St (SR 1462)	E Pritchard St	Asheboro	0.2	20	2	10	60	35	9500	2600	3100	3100	10200	2A	60	Min	Sub	
RAND0060-H	Medowbrook Rd (SR 2184)	E Pritchard St	E Allred Rd (SR 2182)	Asheboro	0.7	20	2	10	60	35	9500	2500	2900	2900	10200	2A	60	Min	Sub	
RAND0061-H	N Cherry St (SR 1443)	NC 42	Sunset Ave (SR 1442)	Asheboro	0.1	22	2	11	-	35	9900	1500	2600	2600	10200	2F	60	Min	Sub	P
	N Church St (SR 1707)	NC 42	Sunset Ave (SR 1442)	Asheboro	0.2	60	4	12	60	35	25500	7000	9500	9500	25500	ADQ	ADQ	Min	Sub	P,T

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	N Cox St	E Presnell St (SR 1462)	NC 42 (E Salisbury St)	Asheboro	0.5	24	2	12	60	35	10200	2500	4500	4500	10200	ADQ	ADQ	Min	Sub	P,T
	N Park St (SR1451)	NC 42	Sunset Ave (SR 1442)	Asheboro	0.2	28	2	12	-	35	10200	3400	5600	5600	10200	ADQ	ADQ	Min	Sub	P,T
	Newbern Ave (SR 2919)	N Hampton Dr	NC 159	Randolph Co.	0.6	20	2	10	60	55	13600	1000	2000	2000	13600	ADQ	ADQ	Min	Sub	
RAND0062-H	Old Cedar Falls Rd (SR 2216)	Martin Luther King Jr Dr. (SR 2189)	E Presnell St (SR 1462)	Randolph Co.	0.8	20	2	10	-	35	9500	1400	2600	2600	10200	2E	60	Min	Sub	B
RAND0062-H	Old Cedar Falls Rd (SR 2216)	E Presnell St (SR 1462)	Henry Country Rd (SR 2215)	Randolph Co.	2.4	20	2	10	-	35	9500	1800	2900	2900	10200	2E	60	Min	Sub	B
	Old Cedar Falls Rd (SR 2216)	Henry Country Rd (SR 2215)	PAB	Randolph Co.	1.9	24	2	12	-	55	14600	3100	4500	4500	14600	ADQ	ADQ	Min	Sub	B
RAND0037-H	Old Cox Rd (SR 2834)	NC 159	Proposed Asheboro Southern Bypass	Randolph Co.	0.4	20	2	10	60	55	13600	2600	4000	4000	14600	2E	60	Min	Sub	B
RAND0037-H	Old Cox Rd (SR 2834)	Proposed Asheboro Southern Bypass	Pine Hill Rd (SR 2824)	Randolph Co.	0.6	20	2	10	60	55	13600	2600	4000	4000	14600	2E	60	Min	Sub	B
RAND0037-H	Old Cox Rd (SR 2834)	Pine Hill Rd (SR 2824)	Old Humble Mill Rd (SR 2830)	Randolph Co.	0.8	20	2	10	60	55	13600	2200	3800	3800	14600	2E	60	Min	Sub	B
RAND0037-H	Old Cox Rd (SR 2834)	Old Humble Mill Rd (SR 2830)	NC 159 Spur	Randolph Co.	1.2	20	2	10	60	55	13600	2200	3800	3800	14600	2E	60	Min	Sub	B
RAND0037-H	Old Cox Rd (SR 2834)	NC 159 Spur	Southern PAB	Randolph Co.	1.9	20	2	10	60	55	13600	2200	3800	3800	14600	2E	60	Min	Sub	B
	Old Farmer Rd (SR 3255)	US 64	I-73/I-74 (US-220 Bypass)	Randolph Co.	2.3	24	2	12	60	35	10200	2200	3800	3800	10200	ADQ	ADQ	Min	Sub	
	Old Farmer Rd (SR 3255)	I-73/I-74 (US-220 Bypass)	Uwharrie St (SR 1443)	Randolph Co.	0.4	24	2	12	60	35	10200	1500	2200	2200	10200	ADQ	ADQ	Min	Sub	
	Old Humble Mill Rd (SR 2830)	Old Cox Rd (SR 2834)	NC 42	Randolph Co.	2.3	20	2	10	60	55	13600	1400	2200	2200	13600	ADQ	ADQ	Min	Sub	

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	Old Lexington Rd (SR 1004)	Western PAB	NC 42 (E Salisbury St)	Randolph Co.	3.4	24	2	12	60	35-55	14600	4900	6500	6500	14600	ADQ	ADQ	Min	Sub	B
RAND0038-H	Old Liberty Rd (SR 2261)	US 220 Bus (Fayetteville St)	E Balfour Ave (SR 1502)	Asheboro	0.7	22	2	11	60	35	9900	6000	7700	7700	10200	2F	60	Min	Sub	
RAND0038-H	Old Liberty Rd (SR 2261)	E Balfour Ave (SR 1502)	W Central Ave (SR 1504)	Asheboro	0.4	22	2	11	60	35	9900	6000	7700	7700	10200	2F	60	Min	Sub	
RAND0038-H	Old Liberty Rd (SR 2261)	W Central Ave (SR 1504)	Central Farm Rd (SR 2263)	Asheboro	0.4	22	2	11	60	35	9900	6000	7700	7700	10200	2F	60	Min	Sub	
RAND0038-H	Old Liberty Rd (SR 2261)	Central Farm Rd (SR 2263)	Hub Morris Rd (SR 2149)	Asheboro	0.3	22	2	11	60	35	9900	4000	6800	6800	10200	2F	60	Min	Sub	
RAND0038-H	Old Liberty Rd (SR 2261)	Hub Morris Rd (SR 2149)	Gold Hill Rd (SR 2183)	Asheboro	0.1	22	2	11	60	35	9900	4000	6800	6800	10200	2F	60	Min	Sub	
RAND0038-H	Old Liberty Rd (SR 2261)	Gold Hill Rd (SR 2183)	Eastern PAB	Asheboro	1	22	2	11	60	35	9900	4000	6800	6800	10200	2E	60	Min	Sub	B
RAND0039-H	Old NC Hwy 49 (SR 1193)	Western PAB	Union Church Rd (SR 1163)	Randolph Co.	1.6	20	2	10	-	55	13600	2700	3500	3500	14600	2A	60	Min	Sub	
RAND0039-H	Old NC Hwy 49 (SR 1193)	Proposed Asheboro Southern Bypass	Union Church Rd (SR 1163)	Randolph Co.	1.4	20	2	10	-	55	13600	4100	4500	4500	14600	2A	60	Min	Sub	
RAND0039-H	Old NC Hwy 49 (SR 1193)	Proposed Asheboro Southern Bypass	NC 49	Randolph Co.	1	20	2	10	-	55	13600	4000	4500	4500	14600	2A	60	Min	Sub	
RAND0044-H	Pine Hill Rd (SR 2824)	Bowers Chapel Rd (SR 2826)	Proposed Asheboro Southern Bypass	Randolph Co.	0.4	20	2	10	60	55	13600	800	1600	1600	13600	2A	60	Min	Sub	
RAND0044-H	Pine Hill Rd (SR 2824)	Proposed Asheboro Southern Bypass	Old Cox Rd (SR 2834)	Randolph Co.	0.8	20	2	10	60	55	13600	800	1600	1600	13600	2A	60	Min	Sub	
	Pineview Rd (SR 1712)	Western PAB	I-73/I-74 (US-220 Bypass)	Randolph Co.	0.7	24	2	12	60	55	14600	1400	2400	2400	14600	ADQ	ADQ	Min	Sub	
	Pineview Rd (SR 1712)	I-73/I-74 (US-220 Bypass)	US 220 Bus (Fayetteville St)	Asheboro	1.1	24	2	12	60	35	10200	7000	8000	8000	10200	ADQ	ADQ	Min	Sub	

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
RAND0045-H	Pisgah Covered Bridge Rd (SR 1114)	Southern PAB	73/I-74 (US-220 Bypass)	Randolph Co.	1.04	18	2	9	-	55	13100	1600	2500	2500	14600	2A	60	Min	Sub	
RAND0045-H	Pisgah Covered Bridge Rd (SR 1114)	73/I-74 (US-220 Bypass)	US 220 Bus (Fayetteville St)	Randolph Co.	0.5	18	2	9	-	55	13100	1400	2500	2500	14600	2A	60	Min	Sub	
RAND00075-H	Proposed Crestview Church Rd Ext	NC 159	Bowers Chapel Rd (SR 2826)	Randolph Co.	1.2	-	-	-	-	-	-	-	3500	3600	14600	2A	60	Min	Sub	
RAND0074-H	Proposed Elm St Connector	Elm St	Meadowbrook Rd	Asheboro	0.2	-	-	-	-	-	-	-	1500	1500	10200	2A	60	Min	Sub	
RAND0071-H	Proposed Northeast Blvd	Hub Morris Rd (SR 2149)	Gold Hill Rd (SR 2183)	Randolph Co.	0.1	-	-	-	-	-	-	-	4500	4500	14600	2A	60	Min	Sub	
RAND0071-H	Proposed Northeast Blvd	Gold Hill Rd (SR 2183)	Giles Chapel Rd (SR 2218)	Randolph Co.	0.8	-	-	-	-	-	-	-	3500	3500	14600	2A	60	Min	Sub	
RAND0071-H	Proposed Northeast Blvd	Henley Country Rd (SR 2215)	US 64/ NC 49 (Dixie Dr)	Randolph Co.	0.5	-	-	-	-	-	-	-	2700	2700	14600	2A	60	Min	Sub	
RAND0076-H	Proposed Pritchard Rd Ext	US 220 Bus (Fayetteville St)	White Oak St (SR 3262)	Asheboro	0.2	-	-	-	-	-	-	-	800	800	10200	2A	60	Min	Sub	
RAND0063-H	Ridge Rd (SR 2915)	US 220 Bus (Fayetteville St)	NC 159	Asheboro	0.4	20	2	10	60	55	13600	4000	6500	6500	14600	2A	60	Min	Sub	
RAND0064-H	S Cherry St (SR 1443)	Sunset Ave (SR 1442)	Dixon Ave (SR 1443)	Asheboro	0.1	22	2	11	-	35	9900	2500	3500	3500	10200	2A	60	Min	Sub	P
	S Church St (SR 1707)	Sunset Ave (SR 1442)	W Kivett St	Asheboro	0.4	60	4	12	60	35	25500	7000	9500	9500	25500	ADQ	ADQ	Min	Sub	T
	S Church St (SR 1707)	W Kivett St	Albemarle Rd (SR 1713)	Asheboro	0.4	60	4	12	50	35	25500	4200	6000	6000	25500	ADQ	ADQ	Min	Sub	T

HIGHWAY

Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	S Church St (SR 1707)	Albemarle Rd (SR 1713)	W Walker Ave (SR 1453)	Asheboro	0.3	60	4	12	50	35	25500	4200	6000	6000	25500	ADQ	ADQ	Min	Sub	T
	S Cox St	NC 42 (E Salisbury St)	E Kivett St	Asheboro	0.5	30	2	12	60	35	10200	3500	6500	6500	10200	ADQ	ADQ	Min	Sub	P,B
	S Cox St	E Kivett St	US 64 /NC 49 (E Dixie Dr)	Asheboro	0.8	30	2	12	60	35	10200	6300	7800	7800	10200	ADQ	ADQ	Min	Sub	P,B
	S Elm St	NC 42 (E Salisbury St)	Cliff Rd	Asheboro	0.2	24	2	12	60	35	10200	2700	4500	4500	10200	ADQ	ADQ	Min	Sub	P
	S Park St (SR1451)	Sunset Ave (SR 1442)	W Kivett St	Asheboro	0.4	28	2	12	-	35	10200	3400	5600	5600	10200	ADQ	ADQ	Min	Sub	P,T
	S Park St (SR1451)	W Kivett St	Albemarle Rd (SR 1713)	Asheboro	0.4	28	2	12	-	35	10200	3400	5600	5600	10200	ADQ	ADQ	Min	Sub	P,T
	S Park St (SR1451)	Albemarle Rd (SR 1713)	W Walker Ave (SR 1453)	Asheboro	0.2	60	4	12	-	35	25500	6500	9100	9100	25500	ADQ	ADQ	Min	Sub	P,T
	S Park St (SR1451)	W Walker Ave (SR 1453)	US 64 /NC 49 (E Dixie Dr)	Asheboro	0.2	60	5	12	-	35	25500	8000	10000	10000	25500	ADQ	ADQ	Min	Sub	P,T
	S Park St (SR1451)	US 64 /NC 49 (E Dixie Dr)	Country Club Rd (SR 1154)	Asheboro	0.1	20	2	10	-	35	9500	1200	2000	2000	9500	ADQ	ADQ	Min	Sub	
	Salisbury St (SR 2237)	NC 42 (Salisbury St)	US 64/ NC 49 (Dixie Dr)	Ashenoro	0.9	24	2	12	100	35	10200	5400	6200	6200	10200	ADQ	ADQ	Min	Sub	B
RAND0047-H	Southmont Dr (SR 1145)	Mack Rd (SR 1144)	73/I-74 (US-220 Bypass)	Randolph Co.	1.4	20	2	10	-	55	13600	1300	2000	2000	14600	2A	60	Min	Sub	
RAND0047-H	Southmont Dr (SR 1145)	73/I-74 (US-220 Bypass)	US 220 Bus (Fayetteville St)	Randolph Co.	1.3	18	2	9	-	55	13100	1400	2000	2000	14600	2A	60	Min	Sub	
RAND0065-H	Spero Rd (SR 1504)	Western PAB	I-73/I-74 (US-220 Bypass)	Randolph Co.	1.4	22	2	11	60	55	14100	2000	2400	2400	14600	2A	60	Min	Sub	
RAND0065-H	Spero Rd (SR 1504)	I-73/I-74 (US-220 Bypass)	W Balfour Ave (SR 1502)	Randolph Co.	0.2	22	2	11	60	55	14100	5200	8000	8000	14600	2A	60	Min	Sub	
	Stutts Rd (SR 1326)	US 64	Cable Creek Rd (SR 1320)	Randolph Co.	0.8	20	2	10	60	55	14600	1000	1800	1800	14600	ADQ	ADQ	Min	Sub	

HIGHWAY

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		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)			
	Stutts Rd (SR 1326)	Cable Creek Rd (SR 1320)	Back Creek Ch Rd (SR 1327)	Randolph Co.	0.6	20	2	10	60	55	14600	1000	1800	1800	14600	ADQ	ADQ	Min	Sub	
	Sunset Ave (SR 1442)	NC 42 (E Salisbury St)	I-73/I-74 (US-220 Bypass)	Asheboro	0.15	60	4D	12	300	45	36600	5000	6000	6000	36600	ADQ	ADQ	Min	Sub	P,B
	Sunset Ave (SR 1442)	I-73/I-74 (US-220 Bypass)	S Cherry St(SR 1443)	Asheboro	0.3	32	2	12	-	35	10200	4500	6000	6000	10200	ADQ	ADQ	Min	Sub	P,B
	Sunset Ave (SR 1442)	S Cherry St (SR 1443)	S Park St (SR 1451)	Asheboro	0.3	32	2	12	-	35	10200	6000	7300	7300	10200	ADQ	ADQ	Min	Sub	B
	Sunset Ave (SR 1442)	S Park St (SR 1451)	Church St (SR 1707)	Asheboro	0.2	32	3	11	-	35	10200	6200	7500	7500	10200	ADQ	ADQ	Min	Sub	B
	Sunset Ave (SR 1442)	Church St (SR 1707)	US 220 Bus (Fayetteville St)	Asheboro	0.2	32	2	12	-	35	10200	6500	8000	8000	10200	ADQ	ADQ	Min	Sub	P,B
RAND0066-H	Tot Hill Farm Rd (SR 1163)	NC 49	Hopewell Friends Rd (SR 1142)	Randolph Co.	3	20	2	10	60	55	13600	1600	2500	2500	14600	2E	59	Min	Sub	B
RAND0066-H	Tot Hill Farm Rd (SR 1163)	Hopewell Friends Rd (SR 1142)	NC 49	Randolph Co.	1.5	20	2	10	60	55	13600	1600	2500	2500	14600	2E	60	Min	Sub	
	Uwharrie St (SR 1443)	Dixon Ave (SR 1443)	Old farmer Rd (SR 3255)	Asheboro	0.3	33	2	12	60	35	10200	3000	4500	4500	10200	ADQ	ADQ	Min	Sub	P
	Uwharrie St (SR 1443)	Old Farmer Rd (SR 3255)	W Kivett St	Asheboro	0.1	33	2	12	-	35	10200	3000	4500	4500	10200	ADQ	ADQ	Min	Sub	P
RAND0067-H	Uwharrie St (SR 1443)	W Kivett St	Albemarle Rd (SR 1713)	Asheboro	0.8	22	2	11	-	35	9900	3300	4800	4800	10200	2F	80	Min	Sub	P
	Vision Dr (SR 2269)	I-73/I-74 (US-220 Bypass)	US 220 Bus (Fayetteville St)	Asheboro	1	48	4D	12	-	45	36600	7800	12000	12000	36600	ADQ	ADQ	Maj	Sub	
	W Balfour Ave (SR 1502)	Spero Rd (SR 1504)	US 220 Bus (Fayetteville St)	Asheboro	0.9	24	2	12	60	35	10200	2500	4100	4100	10200	ADQ	ADQ	Min	Sub	
	W Central Ave (SR 1504)	W Balfour Ave (SR 1502)	US 220 Bus (Fayetteville St)	Asheboro	0.9	24	2	12	60	35	10200	3400	6000	6000	10200	ADQ	ADQ	Min	Sub	
	W Central Ave (SR 1504)	US 220 Bus (Fayetteville St)	Old Liberty Rd (SR 2261)	Asheboro	0.6	24	2	12	60	35	10200	2500	5300	5300	10200	ADQ	ADQ	Min	Sub	

HIGHWAY																					
Local ID	Facility	Section		Jurisdiction	Dist. (mi)	2012 Existing System							2040 Proposed System					CTP Classification	Tier	Proposals for Other Modes	
		From	To			Total Width (ft)	Lanes	Lane Width (ft)	ROW (ft)	Speed Limit (mph)	Existing Capacity (vpd)	2012 Volume	2040 Volume E+C	2040 Volume with CTP	Proposed Capacity (vpd)	Cross-Section	ROW (ft)				
	W Kivett St	US 220 Bus (Fayetteville St)	Cox St	Asheboro	0.1	24	2	12	60	35	10200	5000	6500	6500	10200	ADQ	ADQ	Min	Sub	P	
	W Kivett St	Cox St	Cliff Rd	Asheboro	0.4	24	2	12	60	35	10200	5000	6500	6500	10200	ADQ	ADQ	Min	Sub	P	
	W Presnell St (SR 1462)	I-73/I-74 (US-220 Bypass)	White Oak St	Asheboro	0.8	24	2	12	60	35	10200	6800	7800	7800	10200	ADQ	ADQ	Min	Sub		
	W Presnell St (SR 1462)	White Oak St	US 220 Bus (Fayetteville St)	Asheboro	0.2	24	2	12	60	35	10200	6600	7800	7800	10200	ADQ	ADQ	Min	Sub	P	
	W Walker Ave (SR 1453)	Albemarle Rd (SR 1713)	S Park St (SR1451)	Asheboro	0.2	24	2	12	-	35	10200	2000	3500	3500	10200	ADQ	ADQ	Min	Sub		
	W Walker Ave (SR 1453)	S Park St (SR1451)	S Church St (SR 1707)	Asheboro	0.1	56	4	12	-	35	23500	2000	3500	3500	23500	ADQ	ADQ	Min	Sub		
	W Walker Ave (SR 1453)	S Church St (SR 1707)	US 220 Bus (Fayetteville St)	Asheboro	0.1	40	3	12	-	35	13600	2000	3700	3700	13600	ADQ	ADQ	Min	Sub		
	White Oak St (SR 3262)	W Presnell St (SR 1462)	NC 42 (E Salisbury St)	Asheboro	0.5	35	2	12	60	35	10200	1500	2100	2100	10200	ADQ	ADQ	Min	Sub	P	

PUBLIC TRANSPORTATION AND RAIL

PUBLIC TRANSPORTATION ¹							
Local ID	Facility/ Route	Section (From - To)	Speed Limit (mph)	Distance (mi)	Existing System	Proposed System	Other Modes
					Type	Type	
RAND0001-T	US 220 Bus	Downtown Greensboro(I-73/74) - Asheboro Zoo	20-70	34	Bus	Bus	H
RAND0003-T	RCATS Fixed Deviated Route	US 220 Business - US 64/ NC 49 - NC 42 - Park Street	20-45	15		Bus	H,P,B

¹ Only major public transportation routes and proposals are shown here. For further documentation of the public transportation system, refer to *the Piedmont Authority for Regional Transportation (PART) and Regional Coordinated Area Transportation System (RCATS) websites*.

RAIL												
Local ID	Facility/ Route	Section (From - To)	Class	Speed Limit (mph)	Distance (mi)	Existing System			Proposed System			Other Modes
						Type	ROW (ft)	Trains per day	Type	ROW (ft)	Trains per day	
	Norfolk Southern	High Point MPO - South of Southmont Dr (SR 1145)	I	5-30	14.9	Freight	25-100	<5	--	--	--	--

BICYCLE AND PEDESTRIAN ¹

BICYCLE									
Local ID	Facility/ Route	Section (From - To)	Distance (mi)	Existing System		Proposed System		Other Modes	
				Cross-Section (ft)	lanes	Type	Cross-Section		
RAND0001-H	US 64 /NC 49 (Dixie Dr)	NC 42 - I-73/74 / US 220 Byp	2.6	Concurrent with US 64/ NC 49- see Highway Table					
R-2220	US 64 /NC 49 (Dixie Dr)	I-73/74 / US 220 Byp - Albemarle Road (SR 1713)	0.3	Concurrent with US 64/ NC 49- see Highway Table					
U-3600	US 220 Business (Fayetteville Street)	Northern City Limits- Old Liberty Road (SR2261)	4.2	Concurrent with US 220 Business- see Highway Table					
RAND0001-B	US 220 Business (Fayetteville Street)	Old Liberty Road (SR2261) - East Presnell Street (SR 1462)	1.6	60	5-Apr	Bicycle	5A		
RAND0002-B	US 220 Business (Fayetteville Street)	Worth Street - Sunset Avenue	0.1	48	4	Bicycle	--		
RAND0070-H	NC 42	Dublin Road (SR 2197) - US 64/NC 49 (Dixie Drive)	0.5	Concurrent with NC 42- see Highway Table					
RAND0050-H	NC 42 (Salisbury Street)	Cox Street (SR 2327) - Dublin Road (SR 2197)	0.8	Concurrent with NC 42- see Highway Table					
RAND0013-B	NC 49	US 64 - Mack Road (SR 1144)	0.2	24	2	Bicycle			
RAND0003-B	Albemarle Road (SR 1713)	US 64/NC 49 - Church Street	1.2	22-24	2	Bicycle	2E		
RAND0004-B	Church Street	W Walker Avenue - NC 42 (Salisbury Street)	1.7	60	4	Bicycle	(1)		
RAND0071-H	Hub Morris Road (SR 2149)	US 220 Bus (Fayetteville St) - Old Liberty Road (SR 2261)		Concurrent with Hub Morris Rd- see Highway Table					
RAND0005-B	Mack Road (SR 1144)	US 64/ NC 49 (Dixie Drive) - Hopewell	4.6	24	2	Bicycle	2E		
RAND0038-H	Old Liberty Road (SR 2261)	US 220 Bus (Fayetteville St) - Gold Hill Rd (SR 2183)		Concurrent with Old Liberty Rd- see Highway Table					
RAND0006-B	Park Drive (SR 1462)	I-73/74/ US 220 Byp - Westmont Drive	0.5	24	2	Bicycle	2E		
RAND0007-B	Presnell Street (SR 1462)	I-73/74/ US 220 Byp - Cox Street (SR 2327)	0.1	26	2	Bicycle	2E		
RAND0012-B	Salisbury Street (SR 2237)	NC 42 - US 64/ NC 49 (Dixie Drive)	0.9	24	2	Bicycle	2E		
RAND0008-B	W Balfour Avenue	Rail Roads - Old Liberty Road (SR 2261)	0.7	24	2	Bicycle	2E		
RAND0009-B	W Walker Avenue	Albemarle Road (SR 1713) - Park St (SR 1451)	0.2	24	2	Bicycle	2E		
RAND0009-B	W Walker Avenue	Park St (SR 1451) - Church Street	0.1	54	4	Bicycle	(1)		

BICYCLE								
Local ID	Facility/ Route	Section (From - To)	Existing System			Proposed System		Other Modes
			Distance	Cross-Section		Type	Cross-Section	
			(mi)	(ft)	lanes			
RAND0010-B	Westmont Drive	Park Drive (SR 1462) - Old Lexington Road (SR 1004)	0.3	24	2	Bicycle	2E	
RAND0011-B	White Oak Street (SR 3262)	NC 42 (Salisbury Street) - Presnell Street (SR 2344)	0.5	35	2	Bicycle	2E	

PEDESTRIAN								
Local ID	Facility/ Route	Section (From - To)	Existing System			Proposed System		Other Modes
			Distance	Type	Side of Street	Type	Side of Street	
			(mi)					
R-2220	US 64	NC 49 - proposed Mack Road Extension (U-5305)	0.2	--	--	Sidewalk	Both	H
R-2220	US 64/ NC 49 (Dixie Drive)	I-73/I-74/US220 Bypass - NC 49	0.3	--	--	Sidewalk	Both	H
RAND0001-H	US 64/ NC 49 (Dixie Drive)	0.2 mile west of NC 42 - 0.1 mile east of Vista Parkway (SR 2706)	0.7	--	--	Sidewalk	Both	H
U 3600	US 220 Business	Old Liberty Road - Claude Holden Drive (SR 2124)	4.1	--	--	Sidewalk	Both	H
RAND0001-P	NC 49	US 64/ NC 49 (Dixie Drive) - Oak Leaf Road (SR 1323)	0.4	--	--	Sidewalk	Both	
RAND0002-P	NC 159 (Zoo Parkway)	US 64/ NC 49 (Dixie Drive) - 0.1 mile South of Brownmire Drive (SR 2941)	1.5	--	--	Sidewalk	Both	
RAND0003-P	Albemarle Road (SR 1713)	Uwharrie Street (SR 1445) - NC 49	0.2	--	--	Sidewalk	Both	
RAND0004-P	City View Street (SR 1480)	Summit Avenue - Peachtree Street	0.1	--	--	Sidewalk	Both	
RAND0005-P	East Kivett Street	South Main Street and Glenwood Road (SR 2203)	0.2	--	--	Sidewalk	Both	
RAND0006-P	East Presnell Street (SR 2344)	Greensboro Street - Vance Street.	0.5	--	--	Sidewalk	Both	
RAND0007-P	Executive Way	US 64/ NC 49 (Dixie Drive) the Cul-de-sac Brewer Street and East Presnell Street (SR 2344)	0.1	--	--	Sidewalk	Both	
RAND0008-P	Greensboro Street	Church Street (SR 1707) - US 220 Business (Fayetteville Street).	0.3	--	--	Sidewalk	Both	
RAND0010-P	Mack Road Extension (U-5305)	US 64 - NC 49	0.1	--	--	Sidewalk	Both	
RAND0011-P	Meadowbrook Road	East Presnell Street (SR 2344) - Brewer Street	0.2	--	--	Sidewalk	Both	
RAND0012-P	North Elm Street	Salisbury Street (SR 2237) - Worth Street	0.1	--	--	Sidewalk	Both	
RAND0038-H	Old Liberty Road (SR 2261)	US 220 Business to 0.1 mile east of Little Point Road (SR2145)	2.9	--	--	Sidewalk	Both	H

BICYCLE								
Local ID	Facility/ Route	Section (From - To)	Distance (mi)	Existing System		Proposed System		Other Modes
				Cross-Section		Type	Cross-Section	
				(ft)	lanes			
RAND0013-P	West Bailey Street	North Fayetteville Street(US 220 Business and North Asheboro School Road (SR 1723)	0.7	--	--	Sidewalk	Both	
RAND0014-P	West Balfour Avenue (SR 1502)	US 220 Business to Burmil Road	0.5	--	--	Sidewalk	Both	
RAND0015-P	West Walker Avenue	South Park Street (SR 1451) and Albemarle Road (SR 1713)	0.2	--	--	Sidewalk	Both	
RAND0016-P	Worth Street	Shamrock Road - South High Street.	0.1	--	--	Sidewalk	Both	
RAND0017-P	Cox Street (SR 2327)	US 64/ NC 49 (Dixie Drive) - Oakdale Street	0.3	--	--	Sidewalk	Both	
RAND0018-P	Foust Street	Church Street (SR 1707) - Ross Street	0.1	Sidewalk	North	Sidewalk	South	
RAND0019-P	Miller Street	White Oak Street (SR 3262) - US 220 Business (Fayetteville Street)	0.1	Sidewalk	South	Sidewalk	North	
RAND0020-P	Ross Street	Presnell Street (SR 2237) - Foust Street	0.2	Sidewalk	South	Sidewalk	North	
MULTI-USE PATH								
Local ID	Facility/ Route	Section (From - To)	Distance (mi)	Existing System		Proposed System		Other Modes
				Side of Street	Cross- Section	Side of Street	Cross-Section	

' Only major routes and proposals are shown here. For further documentation of bicycle and pedestrian facilities and proposals, refer to *[insert name of document(s)]*.

(1) 4-Lane Undivided with Bike Lanes



**RZ-13-15: Rezone from CURA6 (Conditional Use High-Density Residential)
to RA6 (High-Density Residential)**

(Nancy Jordan: Mountain Road/Springdale Lane/Lincoln Avenue)

Planning Board Recommendation and Staff Report

Planning Board Recommendation & Comments to City Council

NOTE: Have applicant Certify to Council mailings to all adjoining property owners.

Case # RZ-13
-15

Date 11/4/2013

Applicant Nancv Jordan

Legal Description

The property of Nancy J. Jordan located on the south side of Mountain Road at Springdale Lane and on the west side of Lincoln Avenue, totaling approximately 3.25 acres (+/-) and more specifically identified by Randolph County Parcel Identification Number 7751362459.

Requested Action Rezone from CURA6 (Conditional Use High-Density Residential) to RA6 (High Density Residential)

Existing Zone CURA6

Land Development Plan See Rezoning Staff Report

Planning Board Recommendation

Approve (7-2 vote with one member absent)

Reason for Recommendation

The Planning Board believed that the change from CU-RA6 to RA6 was a minor (reasonable) change and after considering that the standard ordinance provisions will still be in place to regulate any development (thereby protecting the public's interest).

Planning Board Comments

Rezoning Staff Report

RZ Case # **RZ-13-15**

Date 11/4/2013 PB
12/5/13 CC

General Information

Applicant Nancy J. Jordan (H.R. Gallimore, Agent)
Address 334 Cannon Court
City Asheboro NC 27203
Phone 336-629-3011
Location Mountain Road & Springdale Lane extending to Lincoln Avenue
Requested Action Rezone from CURA6 (Conditional Use High-Density Residential) to RA6 (High-Density Residential)

Existing Zone CURA6 **Existing Land Use** Undeveloped property
Size 3.25 acres (+/-) **Pin #** 7751362459

Applicant's Reasons as stated on application

To establish usable zoning in keeping with the surrounding property.

Surrounding Land Use

North Single-family/Two-Family Residential **East** Single-family/Two-Family Residential
South Single-family/Two-Family Residential **West** Church/Single-family residential

Zoning History The property was rezoned from R10 to CURA6 in 1981 (Case Number 82-14) and a Conditional Use Permit was issued for a multi-family development consisting of up to 24 units. The Conditional Use permit was never initiated and is considered expired.

Legal Description

The property of Nancy J. Jordan located on the east side of Mountain Road, the south side of Springdale Lane and extending to the west side of Lincoln Avenue, totaling approximately 3.25 acres (+/-) and more specifically identified by Randolph County Parcel Identification Number 7751362459.

Analysis

1. The property is inside the city limits. All city services are available.
2. Mountain Road and Springdale Lane are city-maintained streets at their intersection. Springdale Lane is shown extending to Corwith Street but is uncompleted. Lincoln Avenue is a state-maintained road.
3. Surrounding land uses include primarily single/two-family dwellings and legal non conforming multi-family uses. The multi-family uses would not be permitted by the current zoning ordinance. A church is also located southwest of the subject property.
4. The RA6 district allows development with a Floor Area Ratio of up to 0.17 by right and 0.22 with a Special Use Permit. If this property were rezoned to RA6, the district would permit a residential (single-family or multi-family) development with up to 24,066 square feet by right and up to 31,145 square feet with a Special Use Permit (based on the size of the property).
5. The property's current CURA6 zoning requires a public hearing and issuance of a conditional use permit prior to any development activity.

Rezoning Staff Report

RZ Case # RZ-13-15

Page 2

Consistency with the 2020 LDP Growth Strategy designations

In reviewing this request, careful consideration is given to each Goal and Policy as outlined in the Land Development Plan. Some Goals and Policies will either support or will not support the request, while others will be neutral or will not apply. Only those Goals and Policies that support or do not support the request will be shown.

Growth Strategy Map Designation	Primary Growth
Proposed Land Use Map Designation	Neighborhood Residential
Small Area Plan	Northwest

LDP Goals/Policies Which Support Request

Checklist Item #12. Property is located outside of the watershed area

Checklist Item #13: The property is located outside of Special Hazard Flood Area.

Checklist Item #15: Rezoning is not located on property with steep slopes (>20 %) (applies to western portion of property)

Rezoning Staff Report

RZ Case # RZ-13-15

Page 3

LDP Goals/Policies Which Do Not Support Request

Goal 3.2: Quality design demanding appropriate scale and context (does not support request).

Checklist Item #1: Rezoning is not compliant with the Proposed Land Use Map.

Checklist Item #3: The property on which the rezoning district is proposed does not fit the description of the Zoning Ordinance. (*Article 200, Section 210, Schedule of Statements of Intent*)

Checklist Item #7: The request is not compatible with the applicable (Northwest) Small Area Plan.

Checklist Item #10: Rezoning is not consistent with Land Category Descriptions

Checklist Item #15: Rezoning is located on property with steep slopes (>20 %) (applies to eastern portion of property)

Recommendation

Deny

Reason for Recommendation

Rezoning the property to RA6 would not comply with the LDP proposed land use map and the "neighborhood residential" description in the LDP, which is primarily intended to recognize existing medium-density, single and two-family residential neighborhoods.

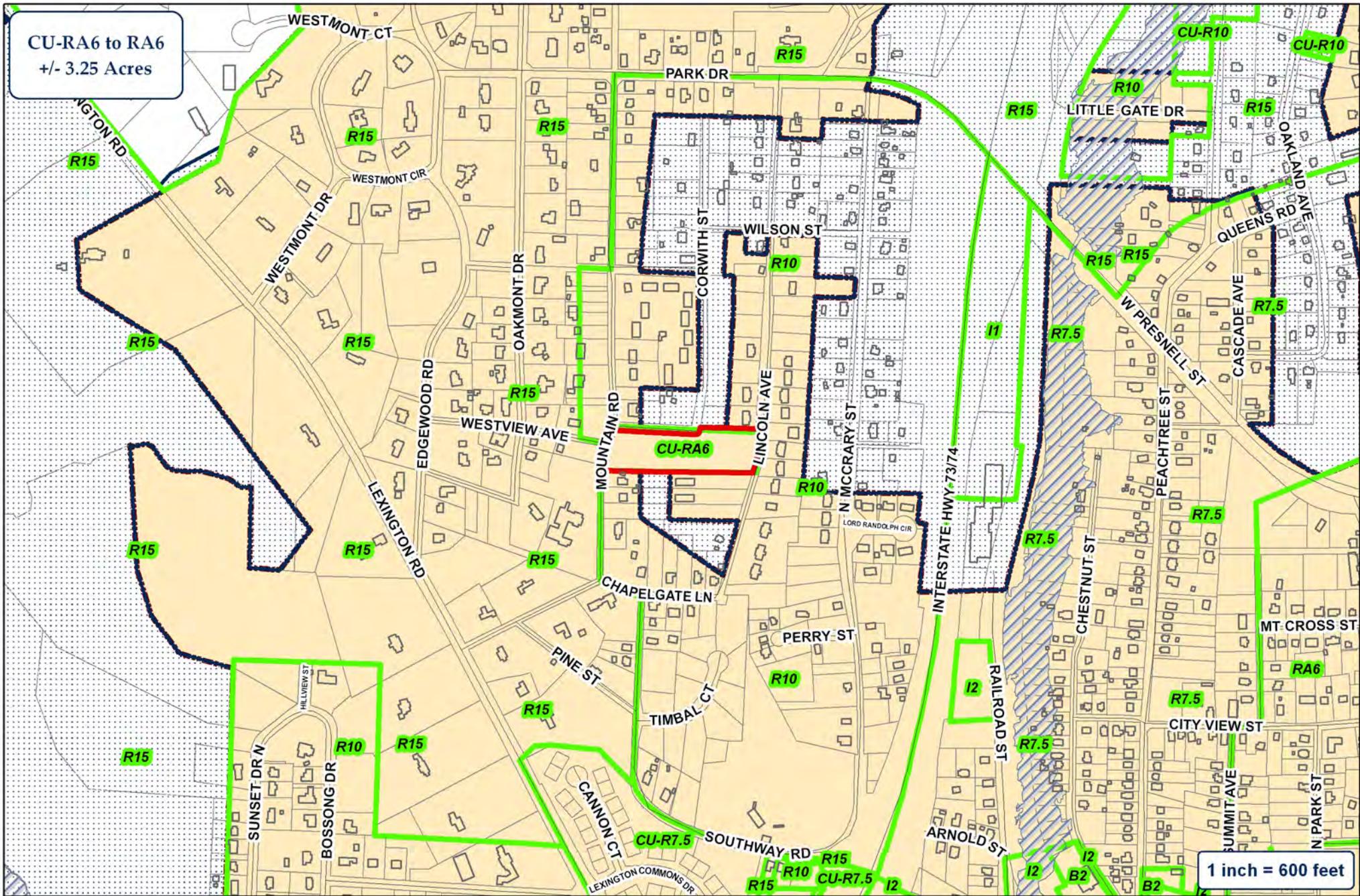
In addition, the entire Northwest Small Area, as defined by the LDP, is absent of RA6 zoning. The NW Small Area Plan characterizes the area as generally lower density residential and does not propose development that would be characteristic of the RA6 High Density Residential District.

In consideration of these factors, the current conditional use permitting process, with the evidence and the oversight required in evaluating development proposals, can help ensure compatible development. However, the absence of this process increases the risk of incompatible development when considering the uses permitted by right and therefore, staff recommends denying the request.

Evaluation of Consistency with Adopted Comprehensive Plans/Reasonableness and Public Interest

Considering the above factors, staff believes that keeping the current Conditional Use High Density Residential (CURA6) zoning is supported by the adopted Land Development Plan and is therefore in the public interest and supports a reasonable use of the property.

CU-RA6 to RA6
+/- 3.25 Acres



City of Asheboro
Planning & Zoning Department
Rezoning Case: RZ-13-15
Parcel: 7751362459

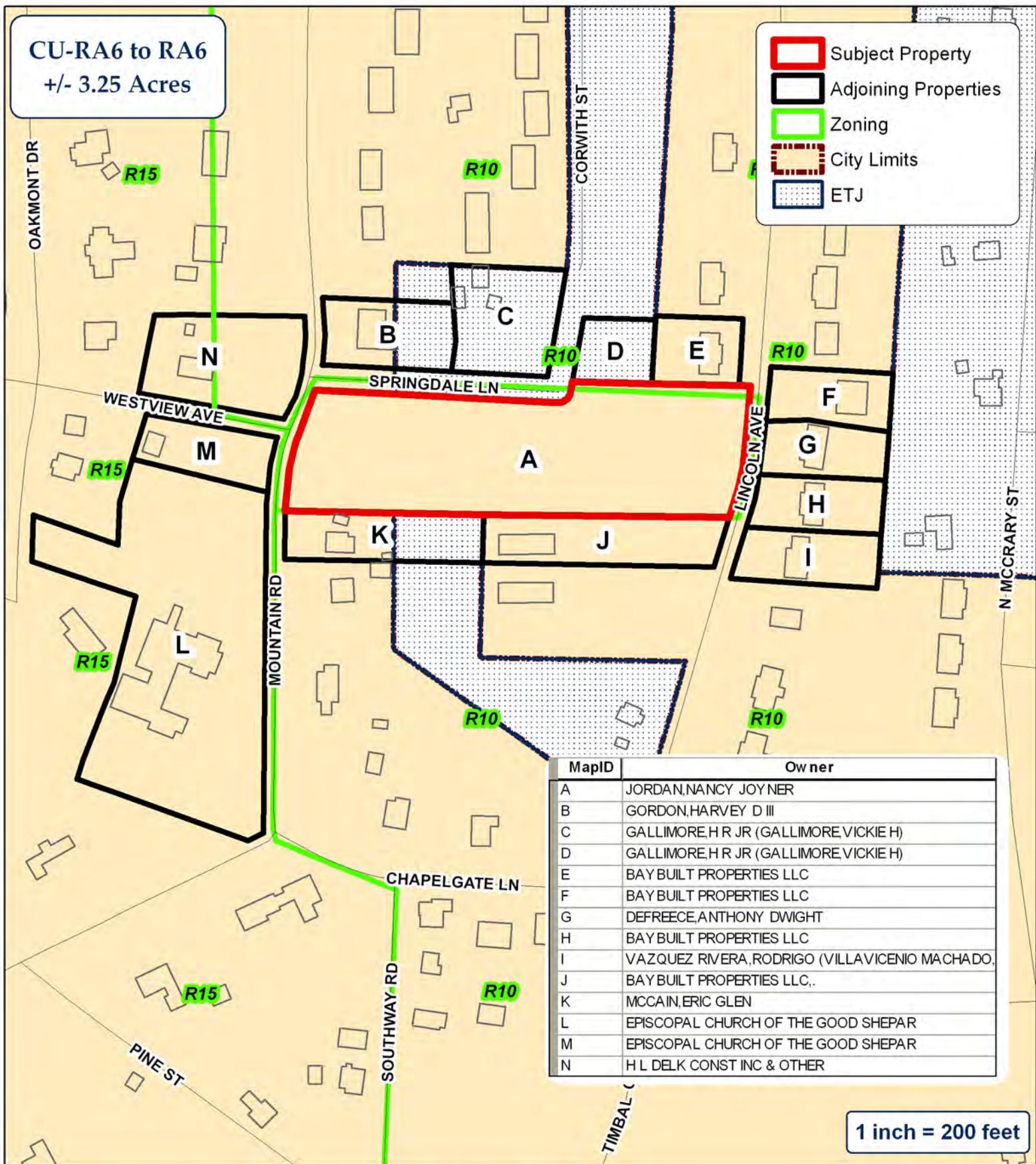
	Subject Property
	Zoning
	City Limits
	ETJ



CU-RA6 to RA6
 +/- 3.25 Acres

Legend

- Subject Property
- Adjoining Properties
- Zoning
- City Limits
- ETJ



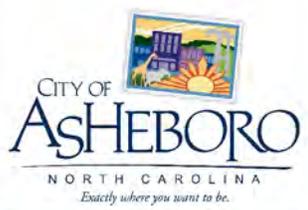
MapID	Owner
A	JORDAN, NANCY JOYNER
B	GORDON, HARVEY D III
C	GALLIMORE, H R JR (GALLIMORE, VICKIE H)
D	GALLIMORE, H R JR (GALLIMORE, VICKIE H)
E	BAY BUILT PROPERTIES LLC
F	BAY BUILT PROPERTIES LLC
G	DEFREECE, ANTHONY DWIGHT
H	BAY BUILT PROPERTIES LLC
I	VAZQUEZ RIVERA, RODRIGO (VILLAVICENIO MACHADO,
J	BAY BUILT PROPERTIES LLC.
K	MCCAIN, ERIC GLEN
L	EPISCOPAL CHURCH OF THE GOOD SHEPAR
M	EPISCOPAL CHURCH OF THE GOOD SHEPAR
N	H L DELK CONST INC & OTHER

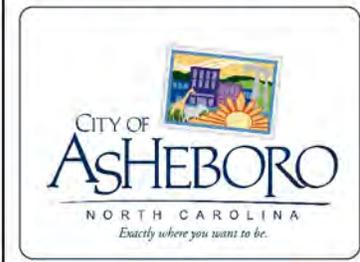
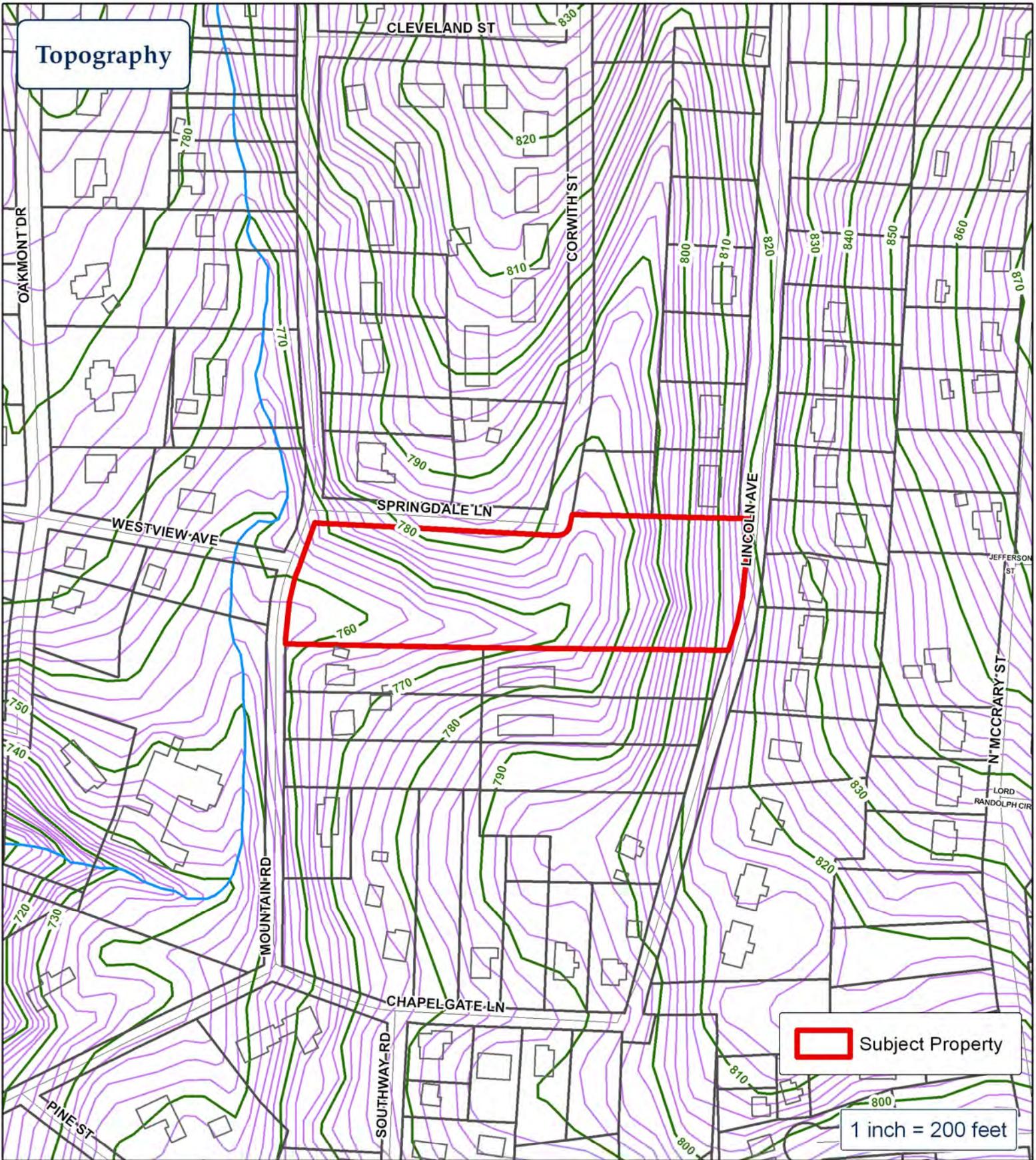
1 inch = 200 feet

City of Asheboro
 Planning & Zoning Department

Rezoning Case: RZ-13-15

Parcel: 7751362459

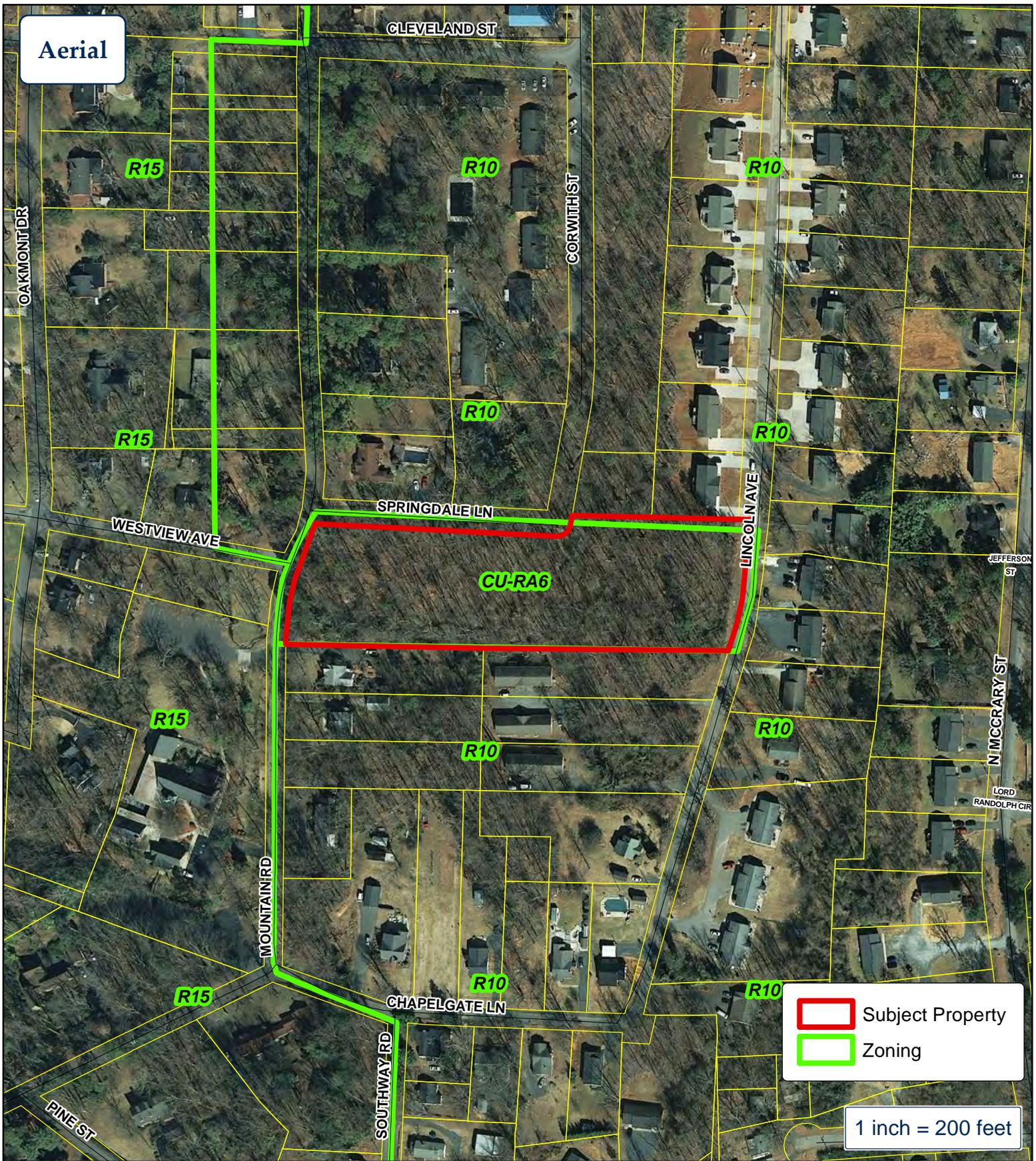




City of Asheboro
Planning & Zoning Department
Rezoning Case: RZ-13-15
Parcel: 7751362459



Aerial



City of Asheboro
Planning & Zoning Department

Rezoning Case: RZ-13-15

Parcel: 7751362459





RZ-13-16: Text Amendments to the Zoning Ordinance:
Board of Adjustment (Article 900)

(City of Asheboro)

Planning Board Recommendation and Staff Report

Planning Board Recommendation & Comments to City Council

NOTE: Have applicant Certify to Council mailings to all adjoining property owners.

Case # **RZ-13**
-16

Date 11/4/2013

Applicant City of Asheboro

Legal Description

Amendments to Article 900 (Board of Adjustment) to reflect changes to the City's Extraterritorial Planning Jurisdiction and the modernization of state statutes regarding Zoning Board of Adjustments.

Requested Action Amend Article 900 of Zoning Ordinance

Existing Zone N/A

Land Development Plan See staff report.

Planning Board Recommendation

Approve.

Reason for Recommendation

The Planning Board concurred with staff reasoning.

Planning Board Comments

Rezoning Staff Report

RZ Case # RZ-13-16

Date 11/4/2013 PB

12/5/13 CC

General Information

Applicant City of Asheboro
Address 146 North Church Street
City Asheboro NC 27203
Phone 336-626-1201
Location N/A

Requested Action Text Amendments to the Zoning Ordinance concerning the Board of Adjustment.

Existing Zone N/A
Existing Land Use N/A
Size N/A
Pin # N/A

Applicant's Reasons as stated on application

The Ordinance is not in error but needs to be revised to reflect changes to the composition of the Planning Board due to reduction in the City's Extraterritorial Planning Jurisdiction and also reflect recent statewide legislation. Article 900 addresses BOA and administrative procedures. Goal 2.1.1 of the LDP encourages periodic review of the Zoning Ordinance. The zoning ordinance needs to reflect current conditions and statutes.

Surrounding Land Use

North N/A
South N/A
East N/A
West N/A

Zoning History

1. The city's extraterritorial zoning jurisdiction boundary was amended, reducing ETJ (effective 1-2013).
2. RZ-13-03: Article 900 was amended to reflect the newly adopted ETJ boundary making members residing outside newly adopted ETJ alternate members (4-2013).

Legal Description

Amendments to Article 900 (Board of Adjustment) to reflect changes to the City's Extraterritorial Planning Jurisdiction and the modernization of state statutes regarding Zoning Board of Adjustments.

Analysis

1. The city's Extraterritorial Planning Jurisdiction boundary was amended effective on January 1, 2013. This reduced the size of the city's Extraterritorial Planning Jurisdiction.
2. In April, 2013, the Zoning Ordinance was amended as a transitional measure to reflect the new geographic boundary of the city's jurisdiction.
3. Recent amendments to state statutes regarding Zoning Board of Adjustments became effective on October 1, 2013. These amendments address stylistic/organizational, technical, and more substantive procedural issues.
4. Procedural changes address the evidentiary tests applicants are required to meet in order to obtain a variance and amend voting requirements in quasi-judicial matters, such as an appeal of a decision made by the Zoning Administrator.
5. The amendments are proposed to address policy changes at both the local and state level related to Board size/composition and procedures.

Rezoning Staff Report

RZ Case # RZ-13-16

Page 2

Consistency with the 2020 LDP Growth Strategy designations

In reviewing this request, careful consideration is given to each Goal and Policy as outlined in the Land Development Plan. Some Goals and Policies will either support or will not support the request, while others will be neutral or will not apply. Only those Goals and Policies that support or do not support the request will be shown.

Growth Strategy Map Designation	N/A
Proposed Land Use Map Designation	N/A
Small Area Plan	N/A

LDP Goals/Policies Which Support Request

2.1.1 The Zoning Ordinance will periodically be reviewed to ensure that the specific regulations for each Zoning District are aligned with the desired character and focus of each district.

Rezoning Staff Report

RZ Case # RZ-13-16

Page 3

LDP Goals/Policies Which Do Not Support Request

Recommendation

Approve

Reason for Recommendation

The amendments are advisable due to changes in both local and state policy related to the Board of Adjustment. As stated by the Land Development Plan and Zoning Ordinance Section 1011.2 (4), ordinance provisions need to be kept current to reflect changing conditions and legislation in order to promote the public health, safety, and general welfare.

Evaluation of Consistency with Adopted Comprehensive Plans/Reasonableness and Public Interest

Considering the above factors, staff believes that the text amendments are reasonable and in the public interest.

ARTICLE 900
BOARD OF ADJUSTMENT &
WATERSHED REVIEW BOARD

901: Establishment of Board of Adjustment & Watershed Review Board

901.1: The Asheboro Planning Board shall serve as the Board of Adjustment and/or the Watershed Review Board for this Ordinance. ~~There shall be two alternate members of the Board of Adjustment. The members appointed as alternate members shall be the most recent appointees to the Planning Board who are not city residents and reside beyond the boundary line for the city's extraterritorial planning jurisdiction as shown on the map entitled "City of Asheboro 2012 Revised Extra Territorial Planning Jurisdiction Area" that is recorded in Plat Book 135, Pages 28-34, Randolph County Registry (Amended 4-4-2013).~~

~~The alternate members of the Board of Adjustment shall be called upon to attend only those meetings and hearings at which one or more members are absent or unable to participate in the hearing of a case because of financial or other interest.~~

901.2: ~~Members of the Planning Board, when acting as the Board of Adjustment and/or Watershed Review Board, shall serve without compensation, but may be reimbursed for direct expenses incurred in connection with the discharge of their duties. Reserved for future amendments.~~

901.3: Members length of term and filling of vacancies shall be as provided for in the Asheboro City Code for members of the Asheboro Planning Board.

901A. Powers of the Watershed Review Board

The powers and duties of the Watershed Review Board are established in Article 300B, Section 325B of the Asheboro Zoning Ordinance.

902: Powers of Board of Adjustment

The Board of Adjustment shall have the following powers:

902.1: Hear and decide appeals from and review any order, requirement, decision, or determination made by the Zoning Administrator or his authorized designee.

902.2: Hear and grant a properly filed request for a variance from the terms of this ordinance where ~~practical difficulties or~~ unnecessary hardships would result

from carrying out the strict letter of this ordinance and where the requested variance serves to observe the spirit of the ordinance, secure public safety and welfare, and do substantial justice. (02/01) Nothing in this Section shall be construed to authorize the Board of Adjustment to permit a use in a district where that use is neither a permitted use nor a permissible Special Use.

902.3: Pass upon, decide or determine such other matters as may be required by this Ordinance.

902.4: To make interpretations of the meaning and intent of this Ordinance in accord with Section 906.

903: Board of Adjustment Administration.

903.1: The Board shall adopt rules of procedures and regulations for the conduct of its affairs.

903.2: All meetings of the Board shall be open to the public.

903.3: The Board shall keep a record of its meetings, including the vote of each member on every question, a complete summary of the evidence submitted to it, documents submitted to it and all official actions.

903.4: The Board shall give notice of matters coming before it by causing a public notice to be placed in a newspaper of general circulation in the City of Asheboro. The notice shall appear once a week for two consecutive weeks, the first insertion to be not less than 15 days prior to the meeting date. Notice also shall be given in accordance with N.C.G.S. 160A-388(a2).

903.5: The person acting as Chairman of the Board or the clerk to the Board is authorized to administer oaths to any witnesses in any matter coming before the Board.

903.6: Applications for variances, requests for interpretations and appeals for review of decisions of the Zoning Administrator, or his authorized designee, shall be filed with the Zoning Administrator, or his authorized designee, as agent for the Board, on forms prescribed by the Zoning Administrator at least 30 days prior to the Board of Adjustment meeting at which the request will be considered. A fully dimensional plat of the property with sufficient information to illustrate the necessity for the variance shall be submitted along with the application. Appeals of decisions of administrative officials charged with enforcement of the Zoning Ordinance shall be filed with the City Clerk no more than 30 days after written or constructive notice of the decision being appealed.

903.7: It shall be the responsibility of the Zoning Administrator, or his authorized designee to notify by certified mail the applicant or appellant of the disposition which the Board made of the matter.

903.8: It shall be the responsibility of the Zoning Administrator, or his authorized designee to issue permits in accord with the Board's action on an appeal or application, if a permit is authorized by the Board action.

903.9: The Zoning Administrator, or his authorized designee shall see to the faithful execution of all portions of the Board's actions, including the enforcement of all conditions which may have been attached to the granting of a variance ~~or approval of a Special Use.~~

904: Quorum and Vote Required

Regular members, on receiving notice of a special meeting which they cannot attend or upon learning that they will be unable to participate in a meeting, shall promptly give notice to the secretary of the Board that they are unable to attend or to participate. ~~On receiving such notice, the secretary, shall, by the most expeditious means, notify an alternate member to attend. At any meeting which they are called upon to attend, alternate members shall have the same powers and duties as regular members.~~

~~Except at the election of officers, at no time shall more than 8 members participate officially in any meeting or hearing.~~

904.1: A quorum of the Board, necessary to conduct any ~~other~~ business of the Board, shall consist of ~~six members~~; a majority of the Board membership. For purposes of this Article, vacant positions on the Board and members who are disqualified from voting on a quasi-judicial matter shall not be considered members of the Board for calculation of the requisite majority.

904.2: The concurring vote of ~~seven members~~ four-fifths of the Board ~~present for the hearing~~ shall be necessary ~~to grant a variance, in order:~~

~~(1) to reverse any order, requirement, decision or determination of the Zoning Administrator, or his authorized designee;~~

~~(2) to decide in favor of the applicant any matter upon which it is required to pass by this Ordinance;~~

~~(3) to approve any application for a variance.~~

904.3: The concurring vote of a majority of the Board shall be necessary ~~to decide any other quasi-judicial matter or to determine an appeal made in the nature of certiorari.~~

904.4 A simple majority vote of the quorum shall be necessary to conduct any other business of the Board.

905: Application of the Variance Power

A variance may be allowed by the Board of Adjustment only in cases involving practical difficulties or unnecessary hardships when substantial evidence in the official record of the application supports all the following findings:

When unnecessary hardships would result from carrying out the strict letter of the zoning ordinance, the board of adjustment shall vary any of the provisions of the ordinance upon a showing of all of the following:

- 905.1: That the alleged hardships or practical difficulties are unique and singular as regards the property of the person requesting the variance and are not those suffered in common with other property similarly located;

Unnecessary hardship would result from the strict application of the ordinance. It shall not be necessary to demonstrate that, in the absence of the variance, no reasonable use can be made of the property.

- 905.2: That the alleged hardships and practical difficulties, which will result from failure to grant the variance, extend to the inability to use the land in question for any use in conformity with the provision of this Ordinance and include substantially more than mere inconvenience and inability to attain a higher financial return.

The hardship results from conditions that are peculiar to the property, such as location, size, or topography. Hardships resulting from personal circumstances, as well as hardships resulting from conditions that are common to the neighborhood or the general public, may not be the basis for granting a variance.

- 905.3: That the variance, if allowed, will not substantially interfere with or injure the rights of others whose property would be affected by allowance of the variance.

The hardship did not result from actions taken by the applicant or the property owner. The act of purchasing property with knowledge that circumstances exist that may justify the granting of a variance shall not be regarded as a self-created hardship.

- 905.4: That the variance is in harmony with and serves the general intent and purpose of this Ordinance and the adopted Land Development Plan.

The requested variance is consistent with the spirit, purpose, and intent of the ordinance, such that public safety is secured, and substantial justice is achieved.

- 905.5: That allowing the variance will result in substantial justice being done, considering both the public benefits intended to be secured by this Ordinance and the individual hardships that will be suffered by a failure of the Board to

~~grant a variance.~~

All of these findings of fact shall be made in the indicated order by the Board of Adjustment, which is not empowered to grant a variance without an affirmative finding of fact on all ~~five~~ four categories above. Each finding of fact shall be supported by substantial evidence in the record of proceedings before the Board.

The Board may impose reasonable appropriate conditions upon the granting of any variance ~~to insure that the public health, safety, and general welfare shall be protected and substantial justice done provided that the conditions are reasonably related to the variance.~~ Violation of such conditions shall be a violation of this Ordinance.

906: Application of Interpretation Power.

An appeal from an order, requirement, decision or determination of the Zoning Administrator, or his authorized designee shall be decided by the Board, based upon its findings of fact and to achieve the intent of the Ordinance. In exercising this power, the Board shall act in a prudent manner so that the purposes of the Ordinance shall be served. The effect of the decision shall not be to vary the terms of the Ordinance nor add to the list of permitted or permissible uses in the districts.

907: Appeal Stays Further Proceedings.

An appeal to the Board of Adjustment from a decision or determination of the Zoning Administrator, or his authorized designee stays all proceedings in furtherance of the decision or determination appealed from, except as provided in Section 908.

908: Exceptions to Stay of Action.

An appeal to the Board of Adjustment of a determination or decision of the Zoning Administrator, or his authorized designee, shall not stay further proceedings in furtherance of the decision or determination appealed from, if the Zoning Administrator, or his authorized designee certifies to the Board after notice of appeal has been filed either:

908.1: That a stay would cause imminent peril to life and/or property.

908.2: That the situation appealed from is transitory in nature and, therefore, an appeal would seriously interfere with enforcement of this Ordinance.

908.3: In each instance, the Zoning Administrator, or his authorized designee, shall place in the certificate facts to support the conclusion.

909: Appeals of Board of Adjustment Actions.

Every quasi-judicial decision of the Board shall be subject to review at the instance of

any aggrieved party by the Superior Court by proceedings in the nature of certioraris. The appeal to Superior Court must be filed within 30 days of the filing by the Secretary of the Board of the decision in the office of the Zoning Administrator or the delivery by the Zoning Administrator, or his authorized designee, of the notice required in 903.7, whichever is later the timeframe prescribed in N.C.G.S. 160A-388.

910:Re-hearings

The Board shall refuse to hear an appeal or application previously denied, if it finds there have been no substantial changes in conditions or circumstances bearing on the appeal or application.

911: Coordination with Flood Damage Prevention Ordinance

When the Board of Adjustment is requested to consider and decide upon any issue within their power which falls within an area of a Flood Damage Prevention Zone, the foregoing regulations and those regulations in Article 700 Flood Damage Prevention Ordinance, Section 704, shall apply except in the case where Article 700 requirements conflict with the requirements of this section. In such situation, Article 700 requirements shall govern.

**RESOLUTION AUTHORIZING THE ENTRY OF THE CITY OF ASHEBORO
INTO AN ONLINE AUCTION MEMO OF UNDERSTANDING**

WHEREAS, Section 160A-270(c) of the North Carolina General Statutes authorizes the City Council of the City of Asheboro to “conduct auctions of real or personal property electronically by authorizing the establishment of an electronic auction procedure or by authorizing the use of existing private or public electronic auction services;” and

WHEREAS, GovDeals, Inc. is an online auction service provider that has extensive experience with the disposal of surplus government property; and

WHEREAS, city staff members have recommended utilizing GovDeals, Inc. on a non-exclusive basis to dispose of city-owned personal property that will be identified with specificity when requests are made at later dates for the Council to declare property to be surplus; and

WHEREAS, in order to implement the proposed online auction procedures, the City of Asheboro must first become a client of GovDeals, Inc.; and

WHEREAS, an Online Auction Memo of Understanding, which is attached to this Resolution as ATTACHMENT A and is hereby incorporated into this Resolution by reference as if copied fully herein, is the legal instrument to establish the city as a GovDeals, Inc. client; and

WHEREAS, the Council agrees with the city staff’s recommendation to enter into the attached Online Auction Memo of Understanding with GovDeals, Inc.;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Asheboro that the establishment of the City of Asheboro as a client of GovDeals, Inc., on a non-exclusive basis, is hereby authorized; and

BE IT FURTHER RESOLVED that the City Manager and all other city officials necessary for the implementation of the proposed online auction are hereby authorized to execute any and all instruments needed to successfully conduct an online auction of surplus property.

This resolution was adopted by the Asheboro City Council in open session during a regular meeting held on the 5th day of December, 2013.

David H. Smith, Mayor

ATTEST:

Holly H. Doerr, CMC, NCCMC, City Clerk

ATTACHMENT A

GovDeals

Online Auction Memo of Understanding

This Online Auction Memo of Understanding (MOU) is between GovDeals, Inc. (“GovDeals”), a Delaware corporation having its principal place of business at 5907 Carmichael Place, Montgomery, Alabama, 36117 and City of Asheboro (“Client”), having its principal place of business at 146 North Church Street – Asheboro, North Carolina 27203.

- 1.0 Description of Services:** GovDeals provides a means for Client to post assets for sale and for potential buyers to bid on these assets via an online auction system. Although GovDeals provides system access for Client to list assets, GovDeals is not a party to the actual sale and has no control over the listed information or the ability of the buyer and Client to complete the transaction.
- 2.0 GovDeals’ Responsibilities:** In addition to maintaining and operating an online auction system, GovDeals will provide Client with the following services for the period agreed to in **5.0** below:
 - 2.1** Access to a GovDeals online “Client Asset Server” (CAS), which will allow Client to load assets to the online auction system, maintain information about assets and view and run reports. The CAS will provide Client with the following capabilities:
 - Accept descriptive information concerning an asset including unlimited photos
 - Allow different auction phases based upon dates and times
 - Allow Client to set minimum starting prices, bid increments and reserves
 - 2.2** Training and support services to assist Client in implementing the GovDeals online auction system, which will include:
 - Familiarization with the nature and operation of CAS
 - Guidance in the posting of assets and provide ongoing support
 - Procedures for taking and posting pictures of assets
 - Based on mutual agreement between GovDeals and Client, training and support services will be provided on-site or via telephone or Internet
 - 2.3** Help Desk support available via telephone or email during normal business hours, except announced holidays.
 - 2.4** Provide marketing of assets posted to the online auction site and promote use of the site to potential buyers.
 - Work with Client to identify items that may benefit from marketing attention.
 - Provide documented proof of all marketing efforts made on behalf of Client.
 - Assist in determining values and starting prices for unique and high dollar assets.
- 3.0 Fees:** Please elect a Flexible Pricing Option (FPO) from **Exhibit A** and enter selection below signature block on MOU page two (2).
- 4.0 Payment:**
 - 4.1** If Client elects to collect auction proceeds, GovDeals will invoice Client for fees on the first business day of the month following the month assets are sold. Client agrees to remit payment to GovDeals within thirty (30) calendar days from receipt of invoice, unless an applicable prompt payment act or similar legislation specifies a different time period.

- 4.2 Client shall promptly, but no more than fifteen (15) business days after the auction end date, notify GovDeals of any transaction that was not completed. The fees for said transaction shall be credited to Client during the next invoice period.
- 4.3 If Client elects GovDeals to collect auction proceeds electronically via PayPal, credit card or wire transfer please review and complete Exhibit B.
- 5.0 **Term of MOU:** This MOU shall commence on the date it is signed by the second party and will continue for a period of twelve months unless otherwise terminated upon sixty days written notice by either party. This MOU shall automatically extend for additional one-year periods, unless either party notifies the other in writing of its intent not to renew at least sixty days prior to the anniversary date.
- 6.0 **Terms and Conditions:** Please find Exhibit C attached as an example of suggested Client Terms and Conditions. At any time during the term of this MOU, Client may modify the Terms and Conditions. Any substitutions or modification must be submitted to GovDeals in writing before posting assets to the GovDeals auction site.
- 7.0 **Governance:** This MOU will be governed, interpreted, construed and enforced in accordance with the laws of the State of North Carolina.
- 8.0 **Non-Exclusive Engagement:** This MOU is not exclusive. Client may utilize other approaches, including traditional auctioneer services or sealed bids. However, it is understood and agreed that Client will not utilize other disposal approaches for an asset at the same time the asset is listed on the GovDeals online auction site or sell by some other means to a prior bidder any item currently or previously listed on the GovDeals site for the purpose of avoiding payment of the GovDeals fee. Client agrees to not manipulate or interfere with the bidding process on the GovDeals site.

E-VERIFY COMPLIANCE SECTION FOR CITY OF ASHEBORO CONTRACTS

During the 2013 session of the North Carolina General Assembly, the legislature enacted the RECLAIM NC Act (S.L. 2013-418; HB786). This Act amended Section 160A-20.1 of the North Carolina General Statutes (the North Carolina General Statutes shall be hereinafter referred to as the "NCGS") by adding subsection (b) to provide, in pertinent part, that "no city may enter into a contract unless the contractor and the contractor's subcontractors comply with the requirements of Article 2 of Chapter 64" of the NCGS. Article 2 of Chapter 64 of the NCGS establishes North Carolina's E-Verify requirements for private employers. For the sole and limited purpose of creating a valid contract with the City of Asheboro, the contractor hereby represents and covenants that the contractor and its subcontractors are, to the degree that the cited statutory provisions are applicable, compliant with the requirements of Article 2 of Chapter 64 of the NCGS. The City of Asheboro is relying upon this E-Verify compliance section in order to enter into this contract. The parties agree to this section only to the extent authorized by law. If this section is held to be unenforceable or invalid in whole or in part, it shall be deemed amended to the extent necessary to make this contract comply with NCGS 160A-20.1(b).

This online auction memo of understanding is agreed to by:

GovDeals, Inc

Client: City of Asheboro

Signature: _____

Signature: _____

Print Name: Robert L. DeBardelaben

Print Name: _____

Title: Operating President

Title: _____

Date: _____

Date: _____

Memo of Understanding Contact:

Attention: Sales Support
5907 Carmichael Place
Montgomery, AL 36117
Telephone Number: 866.377.1494
Fax Number: 334.387.0519
Email: salesupport@govdeals.com

Flexible Pricing Options (FPO)

Select one from options described in **GovDeals Memo of Understanding- Exhibit A:**

Client Collects Proceeds

- Option A1 (7.5% Seller- 0% Buyer)
- Option A2 (0% Seller- 7.5% Buyer)

Client elects FSS (GovDeals collects Proceeds)

- Option B1 (7.5% Seller- 5% Buyer)
- Option B2 (5% Seller- 7.5% Buyer)
- Option B3 (2.5% Seller- 10% Buyer)
- Option B4 (0% Seller- 12.5% Buyer)

Flexible Pricing Options (FPO)

The Client has the option to choose from the following alternative plans:

A - Client Collects Proceeds

Option A1: The Client pays a 7.5%* fee which will be reduced according to the Tiered Fee Reduction Schedule (described below). GovDeals will invoice the client each month for fees on items sold in the previous month. The client is allowed thirty (30) calendar days from date of invoice receipt to remit payment.

Option A2: The Client pays a 7.5%* fee but is given the capability to easily pass the entire fee on to the winning bidder as an Administrative Fee. The client is only allowed to pass on to the winning bidder the amount charged to them based on the tiered pricing schedule above. The amount invoiced to the winning bidder will include the gross sale amount of the item, the administrative fee, any special fees and sales taxes, if any. By passing this fee on to the winning bidder, the client's effective fee is zero percent (0%). GovDeals will invoice the client each month for fees on items sold in the previous month. The client is allowed thirty (30) calendar days from date of invoice receipt to remit payment.

B - Client Elects GovDeals Financial Settlement Services (FSS) allowing GovDeals to Collect Proceeds. Only one option below can be used and once this option is chosen, it cannot be changed for twelve (12) months.

Option B1: The Client pays a 7.5%* fee and the winning bidder pays a 5% Buyers Premium. **

Option B2: The Client pays a 5%* fee and the winning bidder pays a 7.5% Buyers Premium.

Option B3: The Client pays a 2.5%* fee and the winning bidder pays a 10% Buyers Premium.

Option B4: The Client pays zero percent fees (0%) and the winning bidder pays a 12.50% Buyers Premium.

Tiered Fee Reduction Schedule

GovDeals' **Tiered Fee Reduction Schedule** below explains how the base auction fee of 7.5% is reduced for assets that sell in excess of \$100,000 on www.govdeals.com.

- 1. When an asset sells for up to \$100,000 in a winning bid, the GovDeals fee is seven and one-half percent (7.5%*) of the winning bid, but not less than \$5.00.**
- 2. Where an asset sells for more than \$100,000, and up to \$500,000 the GovDeals fee is seven and one-half percent (7.5%) of the winning bid up to \$100,000, plus five and one-half percent (5.5%) of the winning bid for auction proceeds in excess of \$100,000 up to \$500,000.**
- 3. Where an asset sells for greater than \$500,000, and up to \$1,000,000 the GovDeals fee is seven and one-half percent (7.5%) of the first \$100,000 of the winning bid, plus a fee of five and one-half percent (5.5%) of the next \$400,000 of the winning bid, plus a fee of three and one-half percent (3.5%) of the bid amount in excess of \$500,000 up to \$1,000,000.**
- 4. Where an asset sells for greater than \$1,000,000 the GovDeals fee is seven and one-half percent (7.5%) of the first \$100,000 of the winning bid, plus a fee of five and one-half percent (5.5%) of the next \$400,000 of the winning bid, plus a fee of three and one-half percent (3.5%) of the next \$500,000 of the winning bid, plus a fee of two and one-half percent (2.5%) of the bid amount in excess of \$1,000,000.**

*Subject to a minimum per asset/lot fee of \$5.00. **If the Client chooses to pay the full 7.5% fee, they will have access to the **Tiered Fee Reduction Schedule**.

EXHIBIT B - Online Auction Memo of Understanding

Financial Settlement Services (FSS)

It is understood the Client elects GovDeals to collect all proceeds due the Client from the winning bidder and remit the proceeds to the Client less the GovDeals fee. Optionally, the Client may elect to not have GovDeals withhold the fee by electing the appropriate section on the following page of this exhibit.

GovDeals will charge the winning bidder a "Buyer's Premium", therefore, the Client is not allowed to charge the winning bidder an additional "Buyer's Premium".

GovDeals will collect all proceeds from the winning bidder, including the "Buyer's Premium" through PayPal, credit card or wire transfer. This is the only means of payment by the bidder.

The Client will not release an asset to the winning bidder until the Client has received verification from GovDeals that payment has been received from the winning bidder. Prior to an item being released to the winning bidder, the Client will ensure the winning bidder or his/her agent has signed a "Bill of Sale" containing the following notation: "Asset is sold as is, where is and without warranty. Once the asset is removed from the seller's premises there is no refund of monies previously paid". The Bill of Sale must be printed from the Client Asset Server (CAS). Any other "Bill of Sale" used by the Client must be submitted to GovDeals for approval.

No proceeds will be remitted to the Client for any asset sold without verification of payment from GovDeals and verification from the Client the item has been picked up by the winning bidder. Approved payment from the winning bidder through PayPal, credit card or wire transfer will be noted in CAS. It is the Client's responsibility to notify GovDeals when an item has been picked up, which is accomplished by the Client accessing CAS and selecting the "Picked Up" option from the "Paid, not picked up" report.

GovDeals will remit all proceeds collected, less the "Buyer's Premium" and the GovDeals fee to the Client on a weekly basis for all assets marked in CAS as 'Picked Up'. However, if you choose to be invoiced for the GovDeals' fee, GovDeals will remit all proceeds collected, less the "Buyer's Premium" only. All proceeds will be remitted electronically by Automatic Clearing House (ACH) unless elected on the following page of this exhibit to receive a paper check. Whether proceeds are remitted electronically via ACH or via paper check, a detailed backup will be submitted to the Client to support the amount remitted.

Under no circumstance will the Client collect any proceeds directly from the winning bidder and if requested to do so, the Client should refer the winning bidder directly to GovDeals for payment instructions.

GovDeals will absorb all costs of Charge Backs by PayPal or a credit card company where an item is released to the winning bidder after the Client receives proper payment notification from GovDeals, GovDeals receives proper pickup notification from the Client and the Client obtained and retained a signed "Bill of Sale" from the winning bidder.

GovDeals will refund proceeds collected to the winning bidder in those rare occasions where the winning bidder pays for an asset but never picks it up and subsequently convinces PayPal or the credit card company to withdraw the amount from GovDeals' bank account. It is the Client's responsibility to request a credit on the asset paid for but not picked up as soon as the allowable pick up time passes. By taking the credit, it insures GovDeals will not charge the Client a fee and will allow the Client to resell the asset. If the asset is mistakenly placed in 'picked up' status by the Client and GovDeals has remitted payment, the Client agrees to refund this amount back to GovDeals.

A GovDeals' Client Services Representative or a GovDeals Help Desk Representative will train the Client on how to effectively use the Financial Settlement Services feature and provide ongoing support as needed. There are no additional costs to the Client for training and support.

GovDeals is covered by a Crime Insurance Policy with a limit of \$5,000,000, which will protect the Client against any loss of funds.

Financial Settlement Services (FSS) Election and Information

Please complete payment instructions below:

If client elects FSS, this section must be completed when submitting the signed MOU back to GovDeals.

Accounting Contact: _____
(Person to receive checks and invoices) Name and Title

E-Mail Address: _____

Phone Number: _____

(Please choose only one option for payment)

If payment will be made by ACH, please provide the following information:

Name of Bank	
County of Bank	
Name of Client: (Name on bank account)	
Bank Routing Number	
Bank Account Number	
Checking/Savings	

OR:

If payment will be made by paper check, please provide the following information:

Make check payable to: _____
Client's Legal Name

Mail check to: _____
Street Address / P.O. Box Number

City, State and Zip Code

Please check here *only* if Client elects to NOT allow GovDeals to deduct the GovDeals fees from proceeds due the client.

City of Asheboro
Asheboro, North Carolina

Online Sales - Terms and Conditions

All bidders and other participants of this service agree they have read and fully understand these terms and agree to be bound thereby.

Guaranty Waiver. All assets are offered for sale “AS IS, WHERE IS.” City of Asheboro (**Seller**) makes no warranty, guaranty or representation of any kind, expressed or implied, as to the merchantability or fitness for any purpose of the property offered for sale. The Buyer is not entitled to any payment for loss of profit or any other money damages – special, direct, indirect or consequential.

Description Warranty. **Seller** warrants to the Buyer the property offered for sale will conform to its description. Any claim for misdescription must be made prior to removal of the property. If **Seller** confirms the property does not conform to the description, **Seller** will keep the property and refund any money paid. The liability of the **Seller** shall not exceed the actual purchase price of the property. Please note upon removal of the property, **all sales are final.**

Personal and property risk. Persons attending during exhibition, sale or removal of goods assume all risks of damage of or loss to person and property and specifically release the seller and GovDeals from liability therefore.

Inspection. Most assets offered for sale are used and may contain defects not immediately detectable. Bidders may inspect the property prior to bidding. Bidders must adhere to the inspection dates and times indicated in the asset description. See special instructions on each asset page for inspection details.

Consideration of Bid. **Seller** reserves the right to reject any and all bids and to withdraw from sale any of the assets listed.

Buyer’s Certificate. Successful bidders will receive a Buyer’s Certificate by email from GovDeals.

Buyers Premium. If a Buyers Premium is shown on the auction page bidder box, then that amount (expressed as a percentage of the final selling price) will be added to the final selling price of all items in addition to any taxes imposed.

Payment. Payment in full is due not later than **5 business days** from the time and date of the Buyers Certificate. Payment must be made electronically through the GovDeals Website.

Acceptable forms of payment are:

- PayPal
- Wire Transfer
- Visa

- MasterCard
- American Express
- Discover

PayPal and Credit Card purchases are limited to below \$5,000.00. If the winning bid plus applicable taxes, if any indicated, and the buyer's premium, equals more than \$4,999.99, PayPal and Credit Cards may not be used. If Wire Transfer is chosen, a Wire Transfer Transaction Summary page will provide payment and account information. The Wire Transfer must be completed within 5 days.

***The next section (payment) is used if the client collects the proceeds and may be modified to reflect exact forms of payment accepted by the client. The Buyers Premium and Payment section (above) is used only when GovDeals collects proceeds.**

***Payment.** Payment in full is due not later than **5 business days** from the time and date of the Buyer's Certificate. Acceptable forms of payment are:

- U. S. Currency
- Certified Cashiers Check
- Money Order
- Company Check (with Bank Letter guaranteeing funds – **mandatory**)

Checks shall be made payable to: **City of Asheboro**. Payments shall be made at the location listed in the Buyer's Certificate.

Removal. All assets must be removed within **ten (10) business days** from the time and date of issuance of the Buyer's Certificate. Purchases will be released only upon receipt of payment as specified. Successful bidders are responsible for loading and removal of any and all property awarded to them from the place where the property is located as indicated on the website and in the Buyer's Certificate. The Buyer will make all arrangements and perform all work necessary, including packing, loading and transportation of the property. Under no circumstances will **Seller** assume responsibility for packing, loading or shipping. See instructions on each asset page for removal details. A daily storage fee of \$10.00 may be charged for any item not removed within the ten (10) business days allowed and stated on the Buyer's Certificate.

Vehicle Titles. **Seller** will issue a title or certificate upon receipt of payment. Titles may be subject to restrictions as indicated in the asset description on the website.

Default. Default shall include (1) failure to observe these terms and conditions; (2) failure to make good and timely payment; or (3) failure to remove all assets within the specified time. Default may result in termination of the contract and suspension from participation in all future sales until the default has been cured. If the Buyer fails in the performance of their obligations, **Seller** may exercise such rights and may pursue such remedies as are provided by law. **Seller reserves the right to reclaim and resell all items not removed by the specified removal date.**

Acceptance of Terms and Conditions. By submitting a bid, the bidder agrees they have read, fully understand and accept these Terms and Conditions, and agree to pay for and remove the property, by the dates and times specified. These Terms and Conditions are displayed at the top of each page of each asset listed on GovDeals. Special Instructions appearing on the asset page will override certain sections of the terms and conditions.

State/Local Sales and/or Use Tax. Buyers may be subject to payment of State and/or local sales and/or use tax. Buyers are responsible for contacting seller or the appropriate tax office, completing any forms and paying any taxes that may be imposed.

Sales to Employees. Employees of the **Seller** may NOT bid on the property listed for auction.